



**City of Kingston
Report to Heritage Properties Committee
Report Number HP-24-013**

To: Chair and Members of the Heritage Properties Committee
From: Jennifer Campbell, Commissioner, Community Services
Resource Staff: Kevin Gibbs, Director, Heritage Services
Date of Meeting: February 21, 2024
Subject: Application for Heritage Permit
Address: 141 King Street East (P18-182)
File Number: File Number: P18-004-2024

Council Strategic Plan Alignment:

Theme: Corporate business

Goal: See above

Executive Summary:

The subject property with the municipal address of 141 King Street East, known as the Belvedere Hotel, is located midblock between Lower Union Street and West Street on the eastern side of the street approximately 70 metres east of City Park. This three bay, two and a half storey brick building has a high stone foundation, several rear yard additions and a carriage house that extends along much of the rear property line. This property is designated under Parts IV & V of the *Ontario Heritage Act* and is subject to a Heritage Easement Agreement with the City.

An application for alteration under Section 42 of the *Ontario Heritage Act* (P18-004-2024) has been submitted to alter/restore the rear elevation of the main building and carriage house as well as alter the rear yard to support a newly proposed spa and additional hotel units on the property. This application was deemed complete on January 25, 2024. The *Ontario Heritage Act* provides a maximum of 90 days for Council to render a decision on an application to alter a heritage building under Section 42(4). This timeframe will expire on April 24, 2024.

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Upon review of all the submitted materials, as well as applicable policies and legislation, staff recommend approval of the proposed scope of work, subject to the conditions outlined herein.

Recommendation:

That the Heritage Properties Committee supports Council approval of the following:

That alterations to the property at 141 King Street East, be approved in accordance with details as described in the application (File Number: P18-004-2024), which was deemed complete on January 25, 2024 with said alterations to include the restoration/alteration of the rear elevation of the main building and carriage house as well as alter the rear yard, specifically:

1. Rear Elevation of the Main Building:
 - a. A previously bricked in door opening will be reinstated and one existing window opening will be enlarged while extending associated brick headers to support modern doors and/or a window;
 - b. Blinding of two openings while retaining existing surrounds;
 - c. Replacement of an existing garage door with modern doors/windows and metal accents;
 - d. Installation of a new fire pit against the base of the rear elevation;
 - e. Installation of a stainless-steel flue for the associated firepit along the entire height of the rear elevation;
 - f. Attachment of two concrete decks with associated staircases and concrete pillars;
 - g. Installation of six surface mounted down lights;
 - h. Removal of a non original rear elevation chimney;
 - i. Repair of all Period Windows;
 - j. Repair rear elevation masonry, as needed;
2. Carriage House Alterations:
 - a. Installation of new wood doors and aluminum windows in all major existing/proposed openings that face the rear yard;
 - b. Exposure of additional foundation/building wall on the northwestern façade due to adjustments to grade;
 - c. Creation of additional openings on the northwestern façade below existing openings that are in similar dimensions to the existing;
 - d. Extension of an existing window opening on the southwestern elevation to accommodate a door;
 - e. Blinding of an existing window opening on the southwestern elevation with metal charcoal siding;
 - f. Addition of concrete underpinnings along the newly proposed grade;
 - g. Repainting/repair of the wooden frame of the dormer surrounds;
 - h. Replacement of the blinded dormer window with a painted wooden window;

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- i. Replacement of the partially blinded opening along the northern and southern most rear yard facing openings with aluminum modern windows, wooden doors, and/or dark stained wood siding;
 - j. Addition of nine new down lights;
 - k. Installation of a new storage structure with charcoal flat profile metal siding and a concrete base that abuts the carriage house with an associated patio, wood trellis/screen and mechanical unit above;
 - l. The creation of 10 new openings along the rear (eastern) elevation facing Ontario Street that will accommodate steel fire rated windows;
 - m. Repair masonry, as needed;
3. Rear Yard Alterations:
- a. Reduce the grade of the rear yard within the width of the main building to accommodate an updated landscaping strategy;
 - b. Installation of two hot tubs on the northeastern portion of the rear yard;
 - c. Installation of a sauna on the southwestern portion of the rear yard;
 - d. Installation of a new reinforced concrete wall abutting an existing concrete wall;
 - e. Installation of a small concrete retaining wall between the main building and carriage house with charcoal metal louvers and black steel flat bar fencing above;
 - f. Installation of a seating area surrounding the fire pit, various planters and ground-oriented lights; and
 - g. Installation of four new trees;

That the approval of the alterations be subject to the following conditions:

1. That the applicant provides written permission from all property owners whose lands will be altered to support the proposed work prior to this permit being in effect;
2. That the applicant consider best conservation/maintenance practices related to those portions of the property that will be exposed to moisture/temperatures changes or interacts with organic matter;
3. That the applicant consider retaining as much of the rear elevation masonry proposed for removal to support the expanded window opening as possible;
4. That the applicant consider not expanding the voussoirs on the rear elevation to avoid legibility concerns;
5. That the applicant consider the creation of a Temporary Protection Plan in consultation with their retained structural engineer and heritage consultant;
6. That the applicant consider an alternative acceptable cladding for the storage shed as listed in section 5.3.3 in the HCD Plan;
7. That the two blinded windows use recessed brick infill for legibility purposes;
8. That the removed limestone masonry units be retained for future property maintenance;
9. That the finalized colour of wood elements on the carriage house and rear elevation be provided to Heritage Planning staff for review/approval prior to installation;
10. That the finalized lighting strategy, including the location of associated wiring, be provided to Heritage Planning staff for review/approval prior to installation;

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11. That the carriage house and storage structure concrete underpinnings be a colour sympathetic to the limestone patina, while also remaining visually distinct;
12. Should any additional masonry wall openings or roof alterations be required on the carriage house to support the project, that those details shall be provided to Heritage Planning staff prior to alteration for review/approval;
13. That the stainless-steel flue associated with the fire pit does not exceed the height of the mansard roof;
14. That interior/exterior photos of the southwestern elevation of the carriage house and roof top photos of the chimney proposed for removal be provided to Heritage Planning staff prior to their alteration for documentation purposes;
15. That the attachment of the concrete platforms/retaining wall to the rear elevation and the attachment of the carriage house to the addition's concrete foundation use a bond breaker to ensure maximum reversibility;
16. That the new openings on the carriage house that face the rear yard be the same width as the existing openings;
17. That the finalized design/installation strategy of the carriage house windows visible from Ontario Street, the storage shed/trellis and fire pit (and its related water feature) be provided to Heritage Planning staff for review/approval prior to installation;
18. That all repairs to wooden features be done with like materials and match existing features in scale and profile;
19. Should any Period Windows on the rear elevation of the main building require replacement, the applicant shall provide an assessment by a qualified heritage professional that is reviewed/approved by Heritage Planning staff prior to removal;
20. All window works shall be completed in accordance with the City's Policy on Window Renovations in Heritage Buildings;
21. All masonry works shall be completed in accordance with the City's Policy on Masonry Restoration in Heritage Buildings;
22. That all necessary studies, permits and survey information (i.e. Grading Plan, Stormwater Management Plan and Tree Permit, Load Calculation, Down Stream Sewer Assessment, etc.) be completed/provided to the satisfaction of the City prior to commencing related works;
23. That the applicant ensures all structures remain sound during and post construction works;
24. A Building Permit shall be completed, as necessary;
25. All *Planning Act* applications and Pre-Applications shall be completed, as necessary;
26. Heritage Services staff shall be circulated the drawings and design specifications tied to the Building Permit and *Planning Act* applications for review and approval to ensure consistency with the scope of the Heritage Permit sought by this application; and
27. Any minor deviations from the submitted plans, which meet the intent of this approval and does not further impact the heritage attributes of the property, shall be delegated to the Director of Heritage Services for review and approval.

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Authorizing Signatures:

ORIGINAL SIGNED BY COMMISSIONER

**Jennifer Campbell,
Commissioner, Community
Services**

ORIGINAL SIGNED BY CHIEF ADMINISTRATIVE OFFICER

**Lanie Hurdle, Chief
Administrative Officer**

Consultation with the following Members of the Corporate Management Team:

Paige Agnew, Commissioner, Growth & Development Services	Not required
Neil Carbone, Commissioner, Corporate Services	Not required
David Fell, President & CEO, Utilities Kingston	Not required
Peter Huigenbos, Commissioner, Major Projects & Strategic Initiatives	Not required
Brad Joyce, Commissioner, Infrastructure, Transportation & Emergency Services	Not required
Desirée Kennedy, Chief Financial Officer & City Treasurer	Not required

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Options/Discussion:**Description of Application/Background**

The subject property with the municipal address of 141 King Street East, known as the Belvedere Hotel, is designated under Parts IV & V of the *Ontario Heritage Act* and is subject to a Heritage Easement Agreement with the City. An application for alteration under Section 42 of the *Ontario Heritage Act* (P18-004-2024) has been submitted to alter/restore the rear elevation of the main building and carriage house as well as alter the rear yard to support a newly proposed spa and additional hotel units on the property.

All submission materials are available online through the Development and Services Hub (DASH) at the following link, [DASH](#), using “Look-up a Specific Address”. If there are multiple addresses, search one address at a time. Submission materials may also be found by searching the file number.

Reasons for Designation/Cultural Heritage Value

The property is designated under both Parts IV and V of the *Ontario Heritage Act* through Designation By-Law Number 81-50 and the Old Sydenham Heritage Area Heritage Conservation District Plan.

By-Law Number 81-50 provides the following relevant information:

- “In plan, scale and decoration, this 1880 building, designed by Joseph Power, shows the development of the stylish mansion. It was the home first of John Hinds, then of Dr. Kenneth Neander Fenwick, a prominent physician.”

The District Plan Property Inventory Evaluation provides the following relevant information related to this proposal:

- The main building has “[a] mansard roof of tessellated slate [that] is bellcast...”
- “The rear wall has two brick abutting additions, two and a half storeys, with irregular fenestration.”
- “A brick coach house behind the main building at 141 King Street East building was in place by the time that the 1892 fire insurance map was printed.”
- “It contains an embedded gable-end dormer, with a peak service door opening, fronting its forward slanting roof with north-side parapet wall.”
- “A squat rectangular window in three pieces, with wood trim and an ashlar sill, sits below the cornice of the building’s southern end.”
- “Its main level contains four sets of large wood vehicle doors, one of which features uppers windows in eight pieces; the others feature tall wood panels. A smaller fifth opening is located on its north end.”
- “Its south elevation contains a rectangular entranceway door with a plain, slightly-arched wood surround, and a matching upper window on an ashlar sill.”

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The property is considered Significant to the District and is subject to a Heritage Easement Agreement.

The relevant parts of Designation By-Law Number 84-65 and the Old Sydenham Heritage Conservation District Plan Property Inventory Evaluation be found in Exhibit B.

Cultural Heritage Analysis

Staff visited the subject property on January 25, 2024.

141 King Street East is an excellent example of one of the City’s most prominent architects, John Power. This “stylish mansion” has multiple rear additions, but much of its heritage value is concentrated on the elevations visible from King Street East. Further, the rear yard also contains a carriage house on the eastern property line. While the carriage house has heritage value and its rear elevation is clearly visible on Ontario Street, the façade of the carriage house (facing into the rear yard) is nearly impossible to see from King Street East (Exhibits A and D). The requested alterations are proposed on the rear elevation of the main building, the rear yard between the building and carriage house, and all three exterior elevations of the carriage house. The below analysis details best practices, a review of the District Plan, a summary of the proposed alterations, and a discussion on the level of impact associated with the project.

Best Heritage Conservation Practices

“The Standards and Guidelines for the Conservation of Historic Places in Canada” (Standards and Guidelines) provides guidance on best practices regarding visual relationships, exterior form, exterior walls, window/doors, entrances/porches, and masonry that are considered character attributes of the property. The below table organizes the most relevant/important best practices into categories as well as summarizes the guidelines applicable to most categories:

Standards and Guidelines Section Number & Categories		Best Practices Detailed in the Standards and Guidelines
4.1.5, 4.3.1, 4.3.4, 4.3.5, 4.3.6 & 4.5.3	Applicable to Most Below Categories	<ul style="list-style-type: none"> • Understand the original planning/design principle of the building/setting; • Assess the condition of the building/feature/setting early in the project; • Maintain/protect the building/feature/setting through cyclical maintenance work; • Repair the building/feature using recognized conservation techniques (which may include limited like-for-like replacement) and by using a minimal intervention approach; • Protect character-defining elements from accidental damage; and

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		<ul style="list-style-type: none"> • Design a new addition/feature that is compatible in terms of its massing/materials/style/character.
4.1.5	Visual Relationships	<ul style="list-style-type: none"> • Design a new feature when required by a new use that respects historic visual relationships.
4.3.1	Exterior Form	<ul style="list-style-type: none"> • Accommodate new uses in non-character defining interior spaces instead of constructing a new addition; and • Design a new addition that draws a clear distinction between new and old.
4.3.4	Exterior Walls	<ul style="list-style-type: none"> • Retain repairable wall assemblies where possible; and • Modify exterior walls to accommodate an expanded use in a manner that respects the building’s heritage value.
4.3.5	Windows/Doors	<ul style="list-style-type: none"> • Protect/retain sound/repairable windows/doors including their functional/decorative elements.
4.3.6	Entrances/Porches	<ul style="list-style-type: none"> • Retain sound/repairable entrances/porches as well as their functional/decorative elements; • Modify/design a new entrance/porch required by a new use that is compatible with building’s style/era/character; and • Remove/alter a non character-defining entrance/porch from a period other than the restoration period.
4.5.3	Masonry	<ul style="list-style-type: none"> • Retain sound/repairable masonry that contributes to the heritage value of the historic place; and • Use mortars that ensure long-term preservation.

Applicable Local Policy/Guidelines

The Old Sydenham Heritage Area Heritage Conservation District Plan (HCD Plan) identifies heritage attributes for the Old Sydenham Heritage Conservation District (the District) and the King Street Corridor sub-area, as well as details policies/guidelines for the District. Relevant heritage attributes for the District include: dominating rear yards, a general high standard of care for buildings, and its proximity to downtown. Relevant heritage attributes for the King Street Corridor sub-area and the District include: varied ages/styles/types of buildings that display two centuries of architectural styles and are associated with the work of prominent Kingston architects that display a high degree of craftsmanship/design merit.

The HCD Plan also details policies/guidelines related to conservation, additions, and building/landscape alterations that apply to the entire District. On conservation, the Plan provides guidance on regular maintenance (which includes protecting/stabilizing buildings to avoid structural collapse), repointing using heritage appropriate techniques/materials, and using replacement stones to match the original source/profile. Further, it notes that property owners should maintain decorative features via recognized conservation techniques, keep decorative features exposed, maintain Period Windows, preserve original openings/surrounds whenever possible, and ensure that new intake/exhaust/fireplace vents/exhausts are not visible from the street.

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On alterations to buildings/landscapes, the HCD Plan notes that one should understand the history of the property to "...differentiate original construction..." from later alterations. Further, the Plan notes that original elements (like Period Windows) be retained/restored whenever possible, that changes be reversible and as inconspicuous as possible, and that property owners do not alter the location/size/shape of existing windows that are visible/face the street. On cladding and utilities the Plan states that cladding should be distinct from the existing building while also noting acceptable cladding materials for the District, and that utilities be at the rear of the building where access permits.

On additions to buildings/landscapes, the HCD Plan notes that additions are not required to replicate an existing heritage style, that cladding be complementary to but distinct from existing buildings, and that additions are permitted at the rear of mid-block buildings. On landscaping, the Plan notes that existing rear yard trees be conserved where possible and that rear yards "should be left to the discretion of the property owners but should take guidance from... [the Plan]." The next section details the scope of the proposal.

Summary of Project Proposal

The applicant seeks to alter the rear elevation of the main building, the rear yard and the carriage house. The impact analysis will follow the below summary of proposed alterations.

Alterations to rear elevation of residential building include:

1. Adjustment of two openings via opening a previously blinded door and extending the width/height of an existing window to accommodate modern doors and a window;
2. Blinding two openings while retaining their surrounds;
3. Replacement of a garage door with glazing/typical glazed door;
4. Installation of a new fire pit and associated stainless-steel flue abutting the rear wall;
5. Attachment of two concrete decks/associated staircases and pillars;
6. Installation of six surface mounted down lights;
7. Repair of all Period Windows; and
8. Removal of a non-original rear elevation chimney.

Alterations to the rear yard include:

1. Grade changes that will expose more of the façade of the carriage house;
2. A new hot tub on the northeastern corner and a new sauna on the southwestern corner;
3. A new reinforced concrete wall along the northern property line;
4. A new small concrete retaining wall between the main building and carriage house with charcoal metal louvers and black steel flat bar fencing above;
5. Installation of four new trees; and
6. A new seating area surrounding the fire pit, various planters and ground-oriented lights.

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Alterations to the carriage house include:

1. Installation of new wood doors and aluminum windows in all major existing/proposed openings;
2. Revealing additional masonry on the northwestern façade due to changes in grade;
3. Creation of additional openings below the existing in similar dimensions;
4. Extending an existing window opening to accommodate a door and blinding an existing window opening with metal charcoal siding on the southwestern elevation;
5. Installation of new concrete underpinnings at the newly proposed grade;
6. Repainting/repairing the wooden frame of the dormer window;
7. Replacement of the partially/fully blinded openings along the northern and southern sides of the façade (northwestern elevation) with modern aluminum windows, wooden doors, and/or dark stained wood siding;
8. Installation of nine new down lights along the façade (northwestern elevation);
9. Creation of 10 new openings along the rear (eastern) elevation facing Ontario Street to accommodate steel fire rated windows; and
10. Installation of a new storage structure with charcoal flat profile metal siding and a concrete base that abuts the carriage house with an associated patio, wood trellis/screen and mechanical unit above.

Rear Elevation Alteration Impact Analysis

The proposal conforms to the HCD Plan and many of Parks Canada's Standards and Guidelines. The alterations proposed for the rear elevation of the main building are largely reversible, impact a secondary altered elevation of a later addition, retain decorative elements and will be nearly invisible to the public. To support these alterations two openings are proposed to be blinded with brick infill, specifically a door and double window opening on the southern most limestone portion of the rear elevation (Exhibits C and D). The door/window surrounds will be retained. To maintain the integrity and legibility of the original openings, staff are requiring that the applicant recess the brick infill. This change also requires the removal of a double wood window and an infill wooden door (Exhibits C and D). According to the chronology of the property, this rear addition first appears in a 1947 fire insurance plan (Exhibit C). As such, while these openings do have value, their blinding poses a negligible impact to the heritage value of the property.

Another potential impact includes the enlargement/unblinding of a window/door and the extension of related existing voussoirs (Exhibits C and D). While neither alteration will be visible from the public realm, expanding an opening where it is unnecessary (i.e. adding additional width for more glazing) is not best conservation practice. In addition, the extension of the existing voussoirs can impact the opening's legibility as this act can confuse its authenticity. A recommendation for the applicant to consider limiting the opening increase has been added to this approval. Despite the above, the act of unblinding the door will return that opening to where it once existed; however, extending the voussoirs is not recommended. In these new openings, new modern doors and related glazing is proposed. This modern design should assist in distinguishing new from old while not impacting the District.

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The project also calls for several attachment points on the rear elevation that should be completely reversible provided the Masonry Policy is followed. These include the attachment of brackets for the stainless-steel flue, the installation of the fire pit, the two large concrete decks/associated staircases/pillars and the six-surface mounted LED down lights (Exhibit C). Provided these installations are attached to the mortar and the platforms use a bond breaker between the structure and the rear elevation wall, as required in this approval, these works should be completely reversible. Installation details on the proposed fire pit and abutting wall have not yet been provided to Heritage Planning staff. However, the finalized design/installation strategy will be provided to staff for review/approval prior to installation to control for negative impacts on the rear elevation as the current drawings show this installation on both the concrete and limestone foundation walls (Exhibits C and D).

Several proposed alterations on the rear elevation do not pose negative impacts. There are no concerns with the replacement of the garage door with a modern door, glazing and an aluminum louver in the existing opening (Exhibit C). Further, the repair of Period Windows is best practice and should help maintain the rear elevation's heritage value. However, if any are potentially irreparable prior to their replacement the applicant must complete a window assessment by a qualified heritage professional to assess their reparability and, if necessary, recommend a suitably designed replacement that replicates the design of the original to the greatest extent possible. Finally, the applicant seeks to remove a chimney that was a later addition to the rear elevation and is no longer functional (Exhibit C). As the chimney has no design/historical value there are no concerns with its removal provided photos of the existing condition from the roof are provided for record purposes prior to its removal. Finally, the rear elevation will be repointed as needed, which is a positive impact.

Rear Yard Alteration Impact Analysis

Many of the alterations to the rear yard are more permanent as they entail regrading the property, adding structures and water features, installing an existing retaining wall and adding new trees (Exhibit C). The rear yard will also be almost completely capped, presumably in concrete and flagstone with a few planters for trees/flora (Exhibit C). As the rear yard is proposed to have multiple levels, additional details on the project are provided below.

Regrading this property entails digging below existing grade to allow for new openings in/access to the carriage house basement level and pathways within the rear yard to access spa amenities (Exhibit C). By reducing the grade, more of the carriage house masonry/foundation will be revealed and a new pathway, below one of the concrete decks, will abut the lowest openings proposed for the carriage house. However, much of the remaining grade of the rear yard will be above this pathway, but below existing grade (Exhibit C). In addition, a grade reduction is proposed to support a new fire pit and associated seating area as well as two hot tubs and a sauna area (Exhibit C). The impact of these grade reductions on the rear yard is neutral as it is not identified as a heritage attribute, and the spatial relationship between the main building and carriage house remains unchanged. The grade reduction will expose more of the main building and carriage house walls/foundations for those who use the private property, but also allow alterations to the carriage house, which will be discussed in the forthcoming carriage house

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alteration section (Exhibit C). The new seating area against the rear wall of the main building should not pose permanent impacts provided the City's Masonry Policy is followed.

Regarding new structures/installations, the two new hot tubs and sauna will not impact the heritage value of the carriage house as all three are either buffered by additional walls or are setback from the carriage house wall (Exhibit C). However, a wall of the sauna room is proposed to include a portion of the rear elevation of the main building. A change in temperature and moisture on a portion of the building that was meant to form part of the exterior wall could result in rapid deterioration and require additional maintenance. As such, the applicant is encouraged to consider best conservation/maintenance practices related to those portions of the property that will be exposed to moisture/temperature changes. The two hot tubs do not pose any heritage impacts to the main building. However, the applicant is proposing a small water feature that will connect one tub to a small water feature surrounding the fire pit (Exhibit C). The applicant intends on using a waterproof membrane between the area near the fire pit and the wall of the rear elevation to control for potential water infiltration concerns. Provided this concern is addressed, this is a neutral impact.

As the new reinforced concrete wall along the northern property line abuts an existing concrete wall and avoids the masonry/brick of both buildings, this installation does not pose heritage concerns (Exhibit C). The small concrete retaining wall between the main building and carriage house, with charcoal/black details/fencing, will remain behind the width of the rear wall of the main building and not be visible from King Street East. While it will not attach to the carriage house it will abut the limestone masonry of the main building (Exhibits A and C). As such, if attached, the retaining wall will use a bond breaker to allow for increased reversibility. Once implemented, heritage impacts should be minimal.

Finally, various planters are proposed to support four new trees and other flora (Exhibit C). The concrete planters are either setback from both buildings or abut a concrete portion of the main building. While one planting bed is setback from both buildings, another partly abuts the rear masonry wall of the main building (Exhibit C). As such, the applicant is encouraged to consider best conservation and maintenance practices for those areas that are newly exposed to organic matter. In addition, the applicant is proposing ground-oriented lights throughout the rear yard that will not interact with either building. This intervention will not result in negative impacts to heritage attributes.

Carriage House Alteration Impact Analysis

The proposed alterations for the carriage house are organized into two major categories: large/visible impacts and moderate/minor impacts. The large and visible impacts are generally reversible, though they will likely remain for the long term, while the moderate/minor impacts (also reversible in nature) can be removed/modified. These large/visible impacts include: the new openings below the existing; new openings facing Ontario Street; additional exposure of the exterior wall; new concrete underpinnings; and extension of a window opening (Exhibit C).

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While the newly exposed masonry due to grade changes allows for a better appreciation of the northwestern wall of the carriage house, it also necessitates additional structural considerations, such as new concrete underpinnings, to maintain the integrity of the building. The applicant has retained a structural engineer who will supervise the project and assist with the related Building Permit submission/review (Exhibit C). Provided the concrete underpinnings are a sympathetic colour, like the existing limestone, and are structurally sound the additional exposure of the wall is a slight benefit from a visual perspective. Overall, the regrading impacts have a neutral impact to the heritage value of the property.

A portion of the limestone exposed because of regrading will be removed to allow for five new openings below the existing in similar dimensions but will have the same width (Exhibit C). New wood doors with aluminum window systems are proposed in both the existing and net new openings, while new dark stained wood siding is proposed for the garage opening (Exhibit C). To mitigate these impacts, staff have required that any stone removed be salvaged, where feasible, and reused for future projects/maintenance work on the property. In addition, the proposed alteration will promote greater use of the structure and produce an active, functional relationship between the carriage house and the residential building. The existing carriage house was most recently used as a storage area and the new openings, and their related use represent a significant enough alteration that this carriage house will likely not return to its use as a garage/storage area for the foreseeable future. The location of this alteration is not visible to the public and therefore it will not have a demonstrable impact to the cultural heritage value of the District. This change represents a negative, but acceptable impact given the location away from public view, the opportunities for salvage and reuse, and change in use/increased connection to the main building.

Significant/visible alterations are also proposed along the rear (eastern) elevation that faces, and is visible from, Ontario Street that includes 10 new window openings and the extension of a window opening to support a new door. The applicant is proposing to install 10 new steel fire rated windows with metal surrounds, which will result in a total of 10.2 square metres of new openings that make up seven percent of the total rear elevation area (Exhibit C). The proposed window configurations maintain most of the masonry of the building and their modern design should maintain the legibility of new and old. Further, as views to the District are not protected in the HCD Plan and the alteration is on the edge of the District's boundary, there are no impacts to the District beyond the alteration of the building itself (Exhibit A).

The window opening extension on the southwest elevation poses a minor but visible impact as this physical change allows for an increased intensity of use by allowing access to the top of a proposed addition that is partly visible from King Street East (Exhibits A and C). This change will draw increased attention from the public realm. This new opening entails lowering the window sill to allow for what appears to be a nearly completely glazed aluminum door (Exhibit C). This alteration represents a fenestration increase of 1.4 square metres or a four percent increase in the existing opening sizes on this elevation (Exhibit C). Despite the increase in attention to this part of the building, this change will allow for additional recreational space for hotel users while posing relatively minor physical attribute impacts as surround details and the opening width will remain unchanged. Finally, the applicant seeks to repoint the carriage house, as needed

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(Exhibit C). As multiple elevations require repointing, this is a positive and highly visible impact that will enhance and maintain the building's heritage value.

The proposal also details moderate/minor reversible impacts that can likely be removed/modified on a shorter time horizon. These impacts include: installing wood doors and aluminum windows on all existing/proposed openings that face the rear yard; replacement of the partially/full blinded opening for the northern and southern most openings; blinding a window on the southwestern elevation; repainting/repairing the dormer window; installation of nine new down lights; and the installation of a new storage structure with charcoal flat profile metal siding and a concrete base that abuts the carriage house with an associated patio, wood trellis/screen and mechanical unit above (Exhibit C). Of the above detailed alterations, only the new storage structure poses meaningful concerns.

The new structure will contain storage space as well as heating/cooling equipment that would otherwise be stored elsewhere on the property (Exhibit C). Housing systems that can create/direct moisture and regulate temperatures in a historic building can create longer term maintenance or preservation concerns if not appropriately implemented/controlled. As such, the storage of such systems in a modern addition is appropriate. While this structure will be visible from King Street East, it "...is clearly secondary to [and distinct from] the main structure, being lower in height, flat-roofed and clad in flat profile charcoal metal siding" (Exhibit C). While modern metal siding is not noted as an acceptable cladding for new additions in the District, it is not prohibited. The proposed colour and material should complement the proposed alterations as well as make the storage space clearly distinct from the existing building. As a condition of approval, it is recommended that the applicant consider a cladding material on the HCD Plan's acceptable cladding list.

The attachment of the proposed pergola structure will follow the City's Masonry Policy by using non-ferrous metal fasteners (Exhibit C). The new mechanical equipment above the storage structure will also be screened by the pergola so it will not be visible from King Street East (Exhibit C). Finally, the use of a concrete base for the storage structure is appropriate, provided it is coloured to be sympathetic with the limestone patina and is attached to the carriage house with a bond breaker to ensure maximum reversibility. Provided the above designs and best practices are followed, this installation should be completely reversible, not draw excessive attention from the traveling public, and remove a potential maintenance concern related to this project all while increasing the usability of the property.

The remainder of the proposed alterations pose minor to neutral impacts to the building's heritage value. The new wood doors and aluminum windows for all major openings as well as the new dark stained wood siding on the northern most opening, are generally sympathetic to the building's heritage value (Exhibit C). Wood is a historic material that is encouraged throughout the District, the use of aluminum windows (without exterior muntin bars) emphasizes that the alteration is a later addition, and the new windows are almost entirely out of view of the public realm (Exhibits C and D). Similarly, the use of metal charcoal siding to blind the southwestern elevation window demonstrates a later alteration while maintaining the existing opening dimensions (Exhibit C).

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In addition, the proposal calls for unblinding the dormer window while retaining/repairing/repainting the existing wooden frame of the dormer window and installing a wood sash window in the existing opening (Exhibit C). These proposed works represent best conservation practices. While the finalized colour has not been provided, a charcoal/aluminum colour would complement the other proposed alterations while also avoiding colour extremes like black or white. Finally, the proposal calls for the installation of nine new LED down lights (Exhibit C). Provided these are installed in the mortar as detailed in the City's Masonry Policy and the wiring is inconspicuous, this alteration should be completely reversible and sympathetic to the property.

Results of Impact Analysis

Overall, the proposed project mitigates negative impacts where feasible, while also strengthening the connection between the carriage house and the main building. In addition, the project will result in positive impacts to the property such as significant repointing/repairs to both buildings. The new openings below existing grade and those facing Ontario Street pose the most significant impacts to the carriage house but have limited impacts on the District due to the carriage house's location on the property and within the District. Further, the necessary grade changes to support the proposed openings below grade provides an opportunity to reveal more of the masonry of that building. While this project will change the use of the carriage house and rear yard, both will continue to support (as well as strengthen the functional connection to) the Belvedere Hotel.

Staff are of the opinion that the subject application will uphold the heritage conservation objectives set out within the City of Kingston's Official Plan, the Ministry of Citizenship and Multiculturalism's Eight Guiding Principles in the Conservation of Built Heritage Properties, and Parks Canada's Standards and Guidelines for the Conservation of Historic Places in Canada. Broadly, the application will:

- Achieve the goal of Section 7.0 (City of Kingston Official Plan): Conserve and enhance built heritage resources within the City so that they may be accessed, experienced and appreciated by all residents and visitors, and retained in an appropriate manner and setting, as a valued public trust held for future generations;
- Achieve Guiding Principle Numbers 2, 3, 4, 6 & 7:
 - Respect for the original location – Do not move buildings unless there is no other means to save them. Site is an integral component of a building. Any change in site diminishes heritage value considerably.
 - Respect for historical material – Repair or conserve rather than replace building materials and finishes, except where absolutely necessary. Minimal intervention maintains the historical content of the resource.
 - Respect for original fabric – Repair with like materials, to return the resource to its prior condition without altering its integrity.
 - Reversibility – Alterations should be able to be returned to original conditions. This conserves earlier building design and technique. For instance, when a new door

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- opening is put in a stone wall, the original stones are numbered, removed and stored, allowing for future restoration.
- Legibility – New work should be distinguishable from old. Buildings should be recognized as products of their own time, and new additions should not blur the distinction between old and new.
 - Achieve Standards 1, 2, 3, 5, 6, 7, 8, 11 & 12 of Parks Canada's Standards and Guidelines:
 - Conserve the heritage value of a historic place. Do not remove, replace or substantially alter its intact or repairable character-defining elements. Do not move a part of a historic place if its current location is a character-defining element.
 - Conserve changes to a historic place that, over time, have become character-defining elements in their own right.
 - Conserve heritage value by adopting an approach calling for minimal intervention.
 - Find a use for a historic place that requires minimal or no change to its character-defining elements.
 - Protect and, if necessary, stabilize a historic place until any subsequent intervention is undertaken.
 - Evaluate the existing condition of character-defining elements to determine the appropriate intervention needed. Use the gentlest means possible for any intervention. Respect heritage value when undertaking an intervention.
 - Maintain character-defining elements on an ongoing basis. Repair character-defining elements by reinforcing their materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving prototypes.
 - Conserve the heritage value and character-defining elements when creating any new additions to an historic place or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.
 - Create any new additions or related new construction so that the essential form and integrity of a historic place will not be impaired if the new work is removed in the future.

Previous Approvals

P18-182-25-05 - Installation of six solar panels.

P18-051-2020EA - Repairs to slate mansard roof.

P18-016-2023 - Repainting, repointing, repair/replacement of damaged windows/entablature and roof, and installation of a new mod bit flat roof.

P18-098-2023 - Repointing and dismantling/rebuilding the carriage house columns and corners, rebuild/repoint front stairs, repouring/recapping rear wall of main building, add concrete cap to existing concrete patio, and rebuild existing patio retaining wall.

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Comments from Department and Agencies

The following internal departments have commented on this application and provided the following comments:

Utilities Kingston:

Utilities Kingston has no issues or concerns with the Heritage Permit aspect of this application but have the following comment if it comes through for a Minor Variance or Site Plan Control Application the following would apply:

To service this location would require a Direct Connection to a combined sewer. Therefore, Utilities Kingston cannot recommend support of this application until such time as the combined sewers are replaced with separated storm and sanitary sewers, and a downstream sewer assessment to validate the capacity of the sanitary sewers is completed.

Engineering Services:

The proposal indicating altering of the existing grades in the courtyard area, please have the applicant provide a grading plan prepared by a qualified individual with sufficient existing and proposed grades so that it can be determined if the proposed alterations will adversely affect drainage patterns and/or adjacent properties. It should be noted that there aren't any storm sewers on King Street East for any flows from the courtyard to be discharged to.

Based on available information it appears that there may be an existing right of way located on this property registered as instrument number FR359819, please have the applicant upload a copy of the instrument so that it can be confirmed that none of the proposed work will impact any registered right of ways in favor of other properties.

Engineering Services – Noise Review:

Should proposal be subject to any other planning applications a noise study will be required to address potential impacts on the proposal due to stationary and transportation noise sources in the vicinity. If new, expanded and/or relocated stationary noise sources are proposed as part of the development the study will have to assess potential impacts on sensitive uses and/or lands zoned for sensitive uses in the vicinity due to stationary noise associated with the proposal. Existing and proposed noise sources associated with the property would have to be included in the assessment. The study would have to be prepared by a qualified individual with experience in environmental acoustics and demonstrate compliance with NPC-300.

Engineering Services – Storm Water Review:

A Stormwater Management Plan prepared by a qualified individual will be required to describe drainage conditions. Post development flows can not exceed pre development flows. Proposed lot and building form shall be suitable for adequate lot drainage. Proposed changes shall not adversely affect or increase stormwater runoff to the neighboring or the subject lands. Plan

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should have erosion and sedimentation control section. Contact engineering@cityofkingston.ca for any questions additional information.

Kingston Hydro:

Kingston Hydro will need a load calculation.

A service request will be required if an upgrade or if additional meters are needed.

<https://utilitieskingston.com/Electricity/NewServices/ServiceRequest>

Building Services:

For the proposal of the 10 new hotel units in the rear carriage house, we require further information as follows:

- a. Provide all existing services on site and sizing of each; combined storm/sanitary sewer and water.
- b. Is the storm combined or surface drainage?
- c. As adding more fixtures within the existing hotel and the carriage house, please provide all fixtures, fixture units and hydraulic loads in order to determine the capacity and ensure it meets the Ontario Building Code.

Planning Services:

This proposal presents a thoughtful concept to adaptively reuse the carriage house on this heritage property.

Please proceed with a Pre-application submission to verify the planning requirements. The submission should include a concept plan of the entire property and showing key elements including the existing building, the proposed scope of work, easements, etc. and a zoning compliance table. Based on an initial review, this proposal will require a Permission application to expand the hotel use beyond the existing building and may require variances for the proposed build out.

Please note that this proposal may require a Site Plan Control application to address site drainage, servicing, etc. This will be determined as part of the pre-application review.

Forestry Services:

The applicant has indicated that all works are to be limited to the private property. If staging or encroachment into the public realm, specifically the boulevard area along King Street East where city owned tree assets exist is to occur, then a Tree Permit to address tree preservation concerns within the boulevard will need to be acquired. A tree protection plan and tree fencing detail will need to be provided as part of the permit application.

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Consultation with the Heritage Properties Committee

The Kingston Heritage Properties Committee was consulted on this application through the [DASH](#) system. Heritage Services has received comments from four Committee members. The Committee's comments have been compiled and attached as Exhibit E.

Several members noted their concerns with the structural integrity of the carriage house.

Two members noted concerns that as some rear yard work appears to have been completed to date, which either makes it challenging to assess existing impacts or is not in alignment with their review of best practices.

One member noted that they supported the rear yard and planned restoration works.

Another member noted that the 10 new windows facing Ontario Street "may be rationalized as being minimally invasive." This same member cautioned that as the rear wall will be subject to weather conditions, preventative measures are necessary to avoid accelerated deterioration. This same member wished to receive detailed descriptions of how the wall will be preserved.

A further member noted that the regrading and carriage house works create negative spatial organization impacts and visual disturbances that are "incompatible in size, scale, material, style and colour" as historically rear yards and carriage houses did not accommodate such functions/installations/openings. This same member expressed concerns with the modern decks that abut both buildings and the reduction of open space. As such, they recommended accommodating spa functions/installations inside the main building basement. The member continued by noting that the proposed carriage house alterations "removes any sense of its former function...". The member provided alternative grade recommendations (i.e. garden flat) that, they believe, would lessen negative impacts. Further, the member noted the importance of retaining a structural engineer with experience on heritage projects. The member also noted that venting/HVAC systems for the carriage house and main building be as inconspicuous as possible. The member also noted that the square openings facing Ontario Street are not "particularly compatible" and should consider 2/2 or 2/3 light configurations with external muntin bars. The member also noted that different doors should be considered for the carriage house openings that are more historically appropriate. The member also noted that widening a window opening to accommodate an additional glazed side panel should be reconsidered. They also noted that the basement window and door should avoid brick infill and that the firepit flue is a distraction for the main building. Finally, they noted that removed windows should be assessed/preserved on site for reversibility and that documentation of existing conditions should be done before works begin.

Conclusion

Staff recommends the approval of the application File Number (P18-004-2024), subject to the conditions outlined herein, as there are no objections from a built heritage perspective, and no concerns have been raised by internal departments in regards to the Heritage approvals.

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Existing Policy/By-Law:

Standards and Guidelines for the Conservation of Historic Places in Canada (Parks Canada)

Ontario Heritage Act, R.S.O. 1990, C.O. 18 (Province of Ontario)

Ontario Heritage Tool Kit (Ministry of Citizenship and Multiculturalism)

City of Kingston Official Plan

By-Law Number 2023-38 Procedural By-law for Heritage

Old Sydenham Heritage Area Heritage Conservation District Plan – Designating By-Law Number 2015-67

Policy on Masonry Restoration in Heritage Buildings

Policy on Window Renovations in Heritage Buildings

Designation By-Law Number 81-50

Heritage Easement Agreement with the City of Kingston (c. 1985)

Notice Provisions:

Pursuant to Section 42(3) of the *Ontario Heritage Act (OHA)*, notice of receipt of a complete application has been served on the applicant.

Contacts:

Joel Konrad, Manager, Heritage Planning, 613-546-4291 extension 3256

Phillip Prell, Intermediate Planner, Heritage Planning, 613-546-4291 extension 3219

Exhibits Attached:

Exhibit A Mapping Information

Exhibit B Old Sydenham HCD Property Entry & By-Law Number 81-50

Exhibit C Project Designs, Engineer Email & HIS Excerpt

Exhibit D Site Visit Photos

Exhibit E Correspondence Received from the Heritage Properties Committee

Exhibit F Final Comments from the Heritage Properties Committee – February 21, 2024

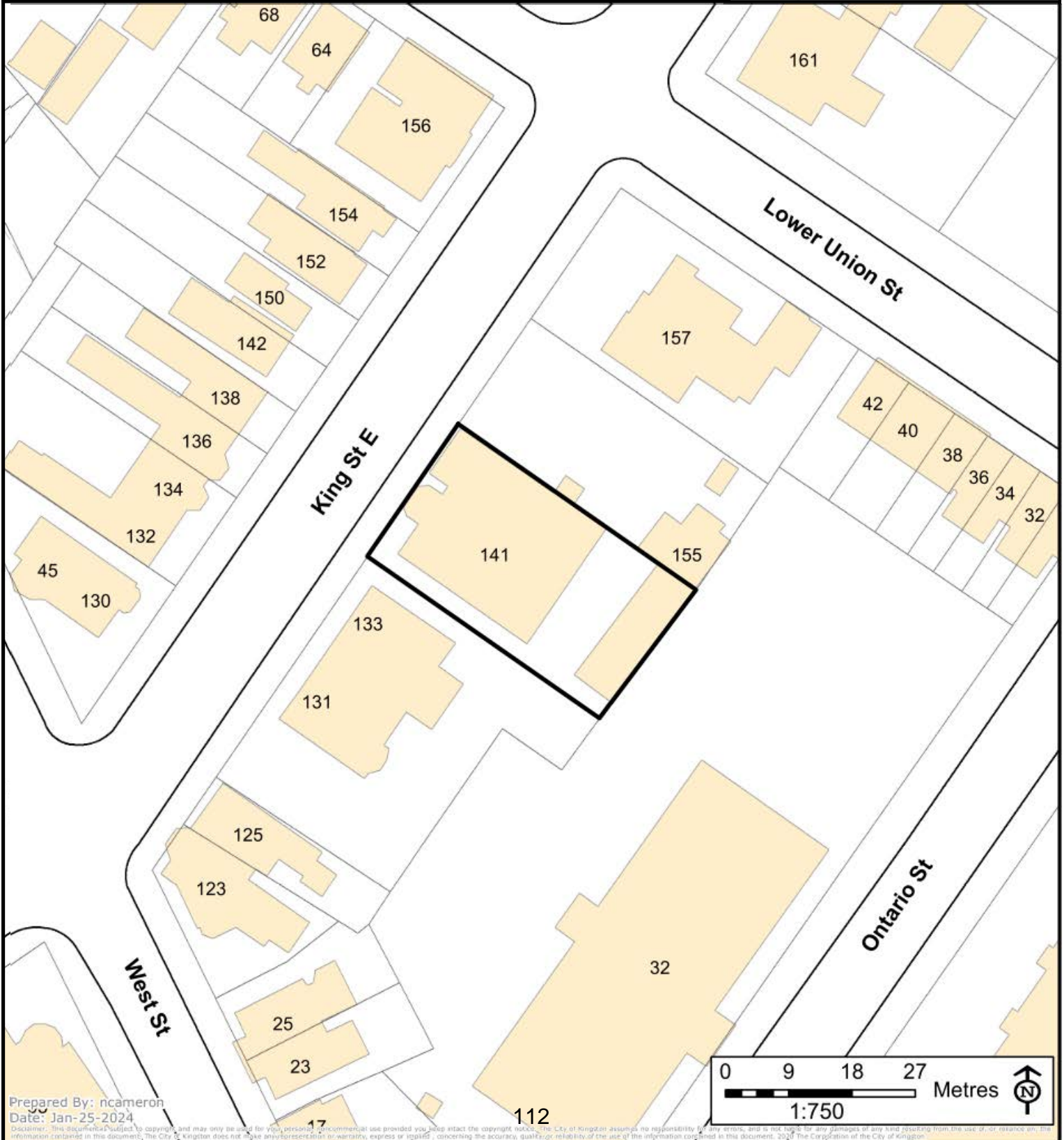
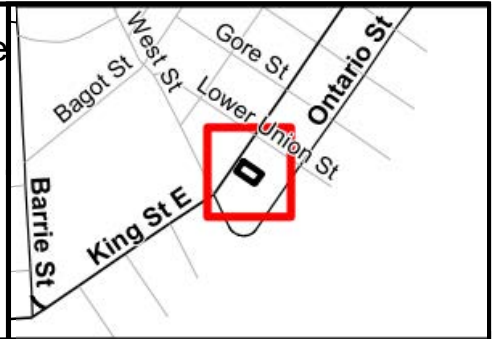


Kingston Heritage Properties Committee

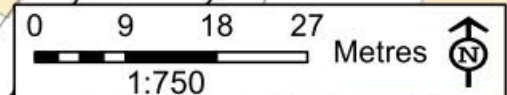
Key Map

Address: 141 King St. E
File Number: P18-004-2024
Prepared On: Jan-25-2024

 Subject Lands



Prepared By: ncameron
Date: Jan-25-2024



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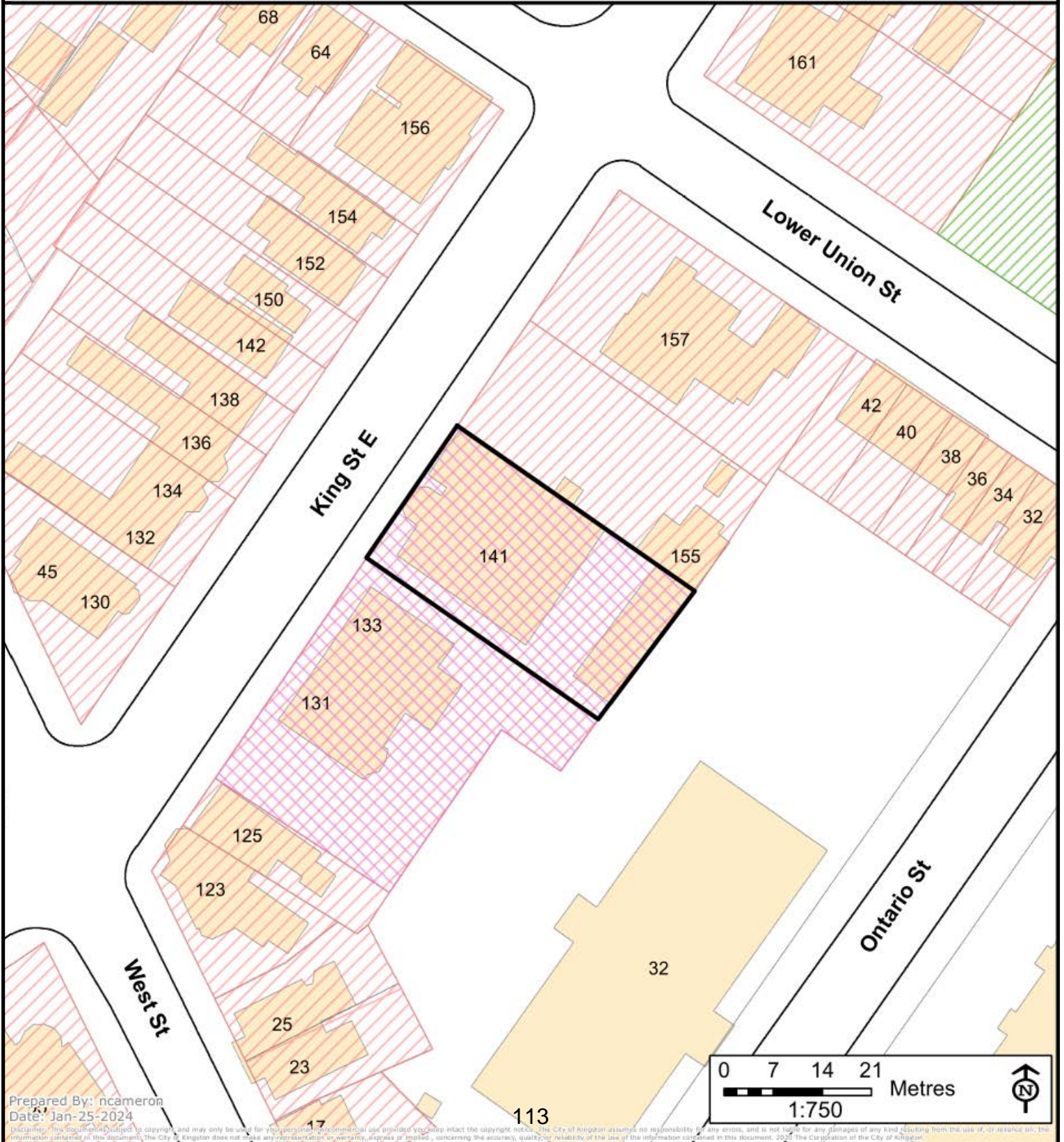


Kingston Heritage Properties Committee

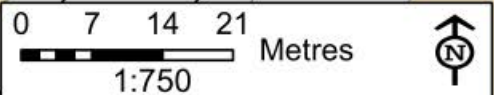
City of Kingston Heritage Register

Address: 141 King St. E
File Number: P18-004-2024
Prepared On: Jan-25-2024

- Subject Lands (Old Sydenham Heritage Conservation District)
- Designated Property
- Easement
- Listed Property






Prepared By: ncameron
Date: Jan-25-2024



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Kingston Heritage Properties Committee
Neighbourhood Context

Address: 141 King St. E
File Number: P18-004-2024
Prepared On: Jan-25-2024

-  Subject Lands
-  Property Boundaries
-  Proposed Parcels



**141 KING STREET EAST
BELVEDERE HOTEL**

Built: 1880

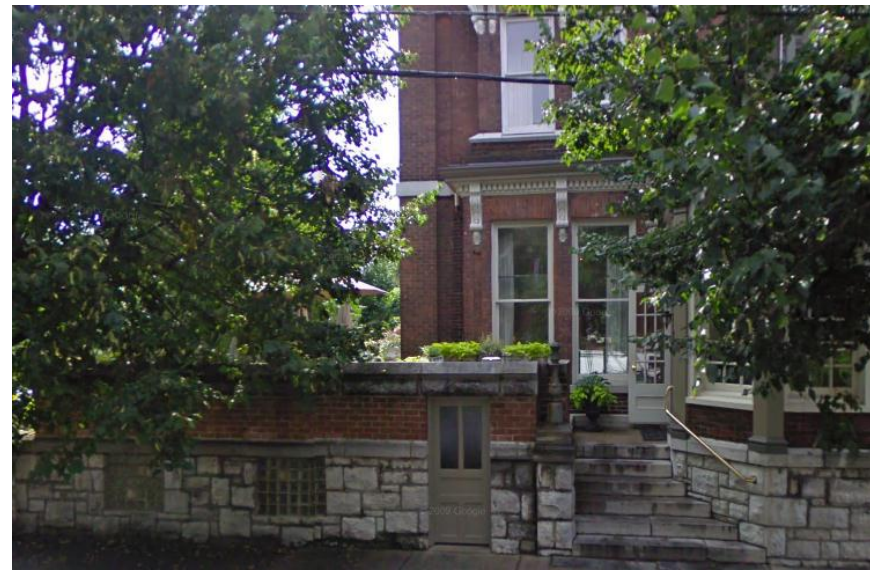
Architect: Joseph Power

Rating: S (Part IV)

Mun. Easement: 1986



“This Victorian building at 141 King Street East is in marked contrast to the plain 1840’s stone building beside it and represents in its plan, scale and decoration, the development of the stylish mansion forty years later. Built for John Hinds, it was sold by his creditors in June, 1883 to Dr. Kenneth N. Fenwick, a prominent physician.



Detail from north-side front entrance

“This three bay, two and a half storey brick building has a high stone foundation irregularly fenestrated. The central bay has a double door enclosed in a classical frame and glass porch. The porch has a brick base resting on a pitch-faced stone front with flanking ashlar quarter turn stairs whose stone newels are topped by cast iron ornaments. A pitch-faced stone wall with a brick top runs from the north staircase to the north perimeter of the property. The porch pilasters support a wide cornice which is topped by a wooden balustrade. The porch windows have multi-light transoms and similar sash.

“Above the porch, the central bay is in a shallow recession and contains a segmental arched doorway with an ashlar keystone.

“The flanking bays have two-storey bay windows; each bay has two windows resting on ashlar courses and each storey has its own roof resting on moulded cornices with dentils and consoles. The first storey

has wide windows flanked by narrow grooves and three large consoles; the second storey has smaller consoles, a single central one flanked by two pairs at each corner.

“A wide round-headed groove at either end of the façade rises two storeys through the ashlar string course. The main cornice has brackets and projects slightly over the bay windows.

“A mansard roof of tessellated slate is bellcast and its cornice has denitls. On the front slope is a flat-roofed dormer flanked by double dormers with broken pediment roofs. All the dormer windows are segmental arched, framed by pilasters and moulded surrounds with keystones.

“The north wall has irregular fenestration with a chimney breast to the front and a two-storey bay window with rectangular ends towards the back. The roof slope has a brick chimney and a wide shingled flat roof dormer with two windows flanking a small rectangular one. The rear wall has two brick abutting additions, two and a half storeys, with irregular fenestration.*

A brick coach house behind the main building at 141 King Street East building was in place by the time that the 1892 fire insurance map was printed. It contains an embedded gable-end dormer, with a peak service door opening, fronting its forward slanting roof with north-side parapet wall. A squat rectangular window in



Coach house

three pieces, with wood trim and an ashlar sill, sits below the cornice of the building’s southern end. Its main level contains four sets of large wood vehicle doors, one of which features uppers windows in eight pieces; the others feature tall wood panels. A smaller fifth opening is located on its north end. Its south elevation contains a rectangular entranceway door with a plain, slightly-arched wood surround, and a matching upper window on an ashlar sill.

* Adapted from *Buildings of Architectural and Historical Significance*, Vol. 5, pp. 156-58 (1980).

Clause 1, Report No. 11, 1981

BY-LAW NO. 81-50

A BY-LAW TO DESIGNATE 131-133 KING STREET EAST, 141 KING STREET EAST AND 157 KING STREET EAST AS PROPERTIES OF HISTORIC OR ARCHITECTURAL INTEREST OR VALUE PURSUANT TO THE ONTARIO HERITAGE ACT, 1974.

PASSED: February 23, 1981

WHEREAS Section 29 of The Ontario Heritage Act, 1974 authorizes the Council of a municipality to enact by-laws to designate real property, including all buildings and structures thereon, to be of architectural or historic value or interest;

AND WHEREAS notices of intention to designate 131-133 King Street East, 141 King Street East and 157 King Street East were served on the owners of 131-133, 141 and 157 King Street East on the 12th day of November, 1980 and were published in the Whig-Standard on November 12th, 19th and 26th, 1980;

AND WHEREAS no notices of objection to the proposed designations have been served on the Clerk of the City of Kingston;

THEREFORE the Council of The Corporation of the City of Kingston enacts as follows:

1. There is designated as being of architectural and historic value or interest the real property known as 131-133 King Street East, in the City of Kingston, as more particularly described in Schedule "A" attached hereto.
2. There are designated as being of architectural value or interest the real properties known as 141 King Street East and 157 King Street East, in the City of Kingston, as more particularly described in the Schedule "A" attached hereto.

3. The City Solicitor is hereby authorized to cause a copy of this by-law and the reasons for designation to be registered against the properties described in Schedule "A" hereto in the Land Registry Office at Kingston, Ontario.

4. The City Clerk is hereby authorized to cause a copy of this by-law to be served on the owners of the properties described in Schedule "A" hereto and on The Ontario Heritage Foundation, and to cause notice of the passing of this by-law to be published in the Whig-Standard once for each of three consecutive weeks.

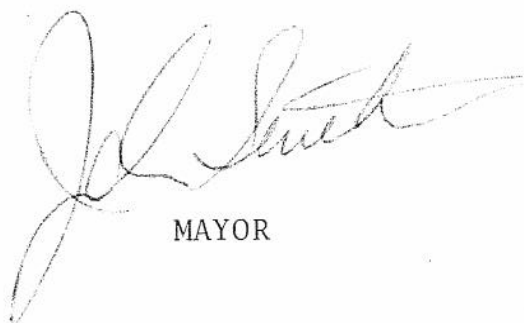
5. This by-law shall come into force and take effect on its passing.

GIVEN FIRST AND SECOND READINGS January 12, 1981

GIVEN THIRD READING AND FINALLY PASSED February 23, 1981



CITY CLERK



MAYOR

SCHEDULE "A"

141 King Street East

ALL AND SINGULAR that certain parcel or tract of land and premises situate, lying and being in the City of Kingston, in the County of Frontenac, in the Province of Ontario, and being composed of Parts of Lots 73, 74, 75 and 78 according to the Original Survey of the City of Kingston, and more particularly described in Instrument No. 106427.

157 King Street East

ALL AND SINGULAR that certain parcel or tract of land and premises situate, lying and being in the City of Kingston, in the County of Frontenac, in the Province of Ontario, and being composed of Parts of Lots 73 and 78 according to the Original Survey of the City of Kingston, and more particularly described in Instrument No. 335660.

131 - 133 King Street East

ALL AND SINGULAR that certain parcel or tract of land and premises, situate lying and being in the Province of Ontario and in the City of Kingston, containing an area of 0.004 hectares and comprising that portion of Town Lot 75 according to the Original Survey of the Town of Kingston (now City of Kingston) designated as PART 1 on a plan of survey made by David T. Humphries, Ontario Land Surveyor, dated the 9th day of February 1981, and deposited in the Land Registry Division of Frontenac (No. 13) on the 11th day of February 1981 as REFERENCE PLAN No. 13R-4483, which said plan forms a part of this legal description.

REASONS FOR DESIGNATION OF
131-133 KING STREET EAST,
141 KING STREET EAST AND
157 KING STREET EAST
APPROVED AT THE MEETING OF CITY COUNCIL
HELD ON JANUARY 12, 1981 (Clause 1, Report No. 11),
AS PROPERTIES OF HISTORIC AND/OR ARCHITECTURAL VALUE OR INTEREST

131-133 KING STREET EAST (Excluding Rear Wing of 131-133 King Street East
and Including Stone Double Dwelling Only)
(Historic and Architectural Significance)

Erected in 1842-43, to plans by William Coverdale for Noble Palmer, founder of the Kingston Spectator, this stone double dwelling served as a temporary residence for Governor General Sir Charles Metcalfe. It was later occupied by the Kingston Seminary for Young Ladies. No. 133 retains its original windows.

141 KING STREET EAST (Belvedere Hotel)
(Architectural Significance)


In plan, scale and decoration, this 1880 building, designed by Joseph Power, shows the development of the stylish mansion. It was the home first of John Hinds, then of Dr. Kenneth Neander Fenwick, a prominent physician.

157 KING STREET EAST (Architectural Significance)

This large brick Victorian mansion, built for Dr. Edward Horsey in 1882, is an excellent example of the work of architect Joseph Power. The dwelling served for many years as a government residence for commanding officers.

I HEREBY CERTIFY THAT the following is a true and correct copy of Clause 1, Report No. 11, 1981, which was approved by the Council of The Corporation of the City of Kingston at their meeting held January 12, 1981:

DATED at Kingston, Ontario,
this 19th day of March, 1981.


M. C. Healy,
Clerk,
The Corporation of the
City of Kingston.

"WHEREAS, on November 12, 1980, Council gave notice of its intention to pass a by-law to designate certain properties to be of historic or architectural value or interest, pursuant to Section 29 of The Ontario Heritage Act, 1974, which properties are more particularly described in Schedule 'A', 1 Page, attached hereto; and

WHEREAS objections to such designation have been received, only from the owner of 131-133 King Street East to designating the rear (east) wing of these buildings (which was not intended to be in the description of the property by L.A.C.A.C.);

THEREFORE BE IT RESOLVED:

- (A) That Schedule 'A' attached be amended in Subsection (a) to add after the words 'Original Survey' the words 'excluding rear wing of 131-133 King Street East and including stone double dwelling only';
- and further -
- (B) That a by-law be presented to Council pursuant to the provisions of The Ontario Heritage Act, 1974 to designate all of the properties, as follows, to be of historic or architectural value or interest for the reasons as listed on Schedule 'A':

141 King Street East (Belvedere Hotel)
157 King Street East
131-133 King Street East (stone double
dwelling only) "

- and -

IN THE MATTER OF THE LANDS AND PREMISES
KNOWN MUNICIPALLY AS 131-133, 141 AND
157 KING STREET EAST, IN THE CITY OF
KINGSTON, IN THE PROVINCE OF ONTARIO

NOTICE OF INTENTION TO DESIGNATE

1. The Council of The Corporation of the City of Kingston intends to pass a by-law to designate the following properties to be of historic and/or architectural value or interest, pursuant to Section 29 of *The Ontario Heritage Act, 1974*:
 - (a) 131-133 KING STREET EAST - Part of Lots 75, 76 & 77, Original Survey (Excluding rear wing of 131-133 King Street East and including stone double dwelling only) (Historic and Architectural Significance)

Erected in 1842-43, to plans by William Coverdale for Noble Palmer, founder of the Kingston Spectator, this stone double dwelling served as a temporary residence for Governor General Sir Charles Metcalfe. It was later occupied by the Kingston Seminary for Young Ladies. No. 133 retains its original windows.
 - (b) 141 KING STREET EAST (BELVEDERE HOTEL) - Part of Lots 73, 74, 75 & 78, Original Survey (Architectural Significance)

In plan, scale and decoration, this 1880 building, designed by Joseph Power, shows the development of the stylish mansion. It was the home first of John Hinds, then of Dr. Kenneth Neander Fenwick, a prominent physician.
 - (c) 157 KING STREET EAST - Part of Lots 73 & 78, Original Survey (Architectural Significance)

This large brick Victorian mansion, built for Dr. Edward Horsey in 1882, is an excellent example of the work of architect Joseph Power. The dwelling served for many years as a government residence for commanding officers.
2. Any person interested may, within thirty (30) days after the date of this notice, send by registered mail or deliver to the Clerk of The Corporation of the City of Kingston notice of his or her objection to the proposed designation, together with a statement of the reasons for the objection and all relevant facts.
3. Where a notice of objection has been received, the Council of The Corporation of the City of Kingston shall refer the matter to the Conservation Review Board for a hearing and report.

LAST DAY TO FILE OBJECTIONS: December 12th, 1980

DATED at Kingston, Ontario,
this 12th day of November, 1980.

M. C. Healy,
Clerk,
The Corporation of the City of Kingston,
City Hall, 216 Ontario Street,
Kingston, Ontario. K7L 2Z3



Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.



Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.

The Belvedere - Courtyard and Carriage House - View 02 02 of 09
Shoalts and Zaback Architects Ltd. **sza**

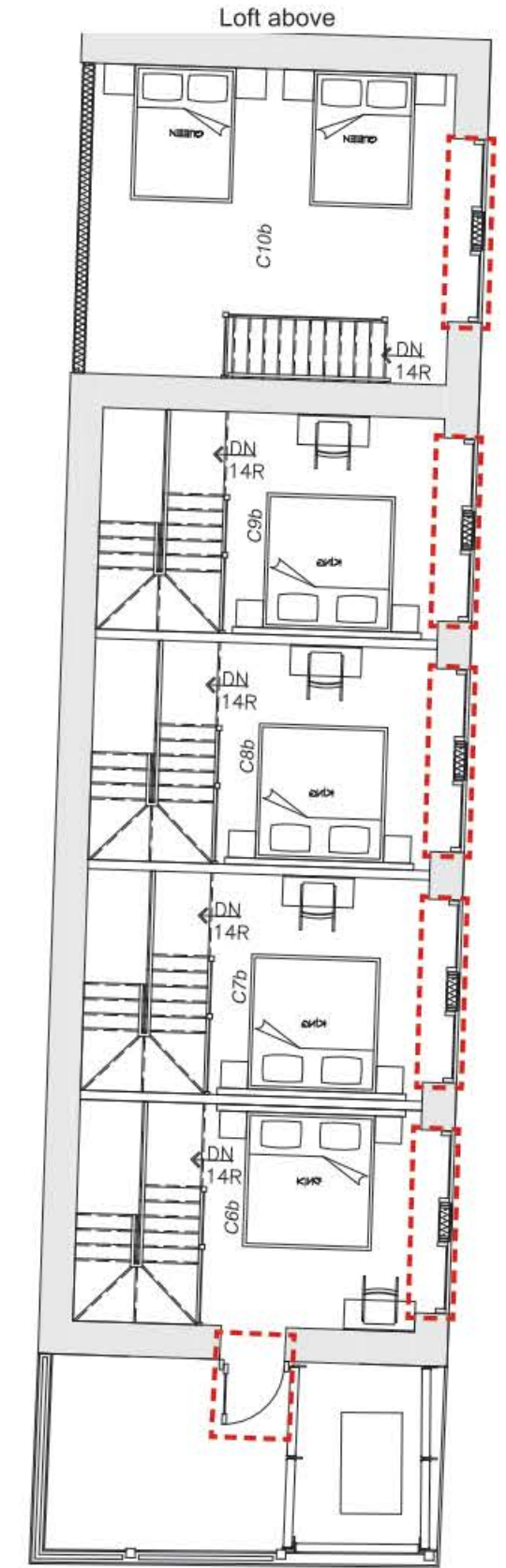
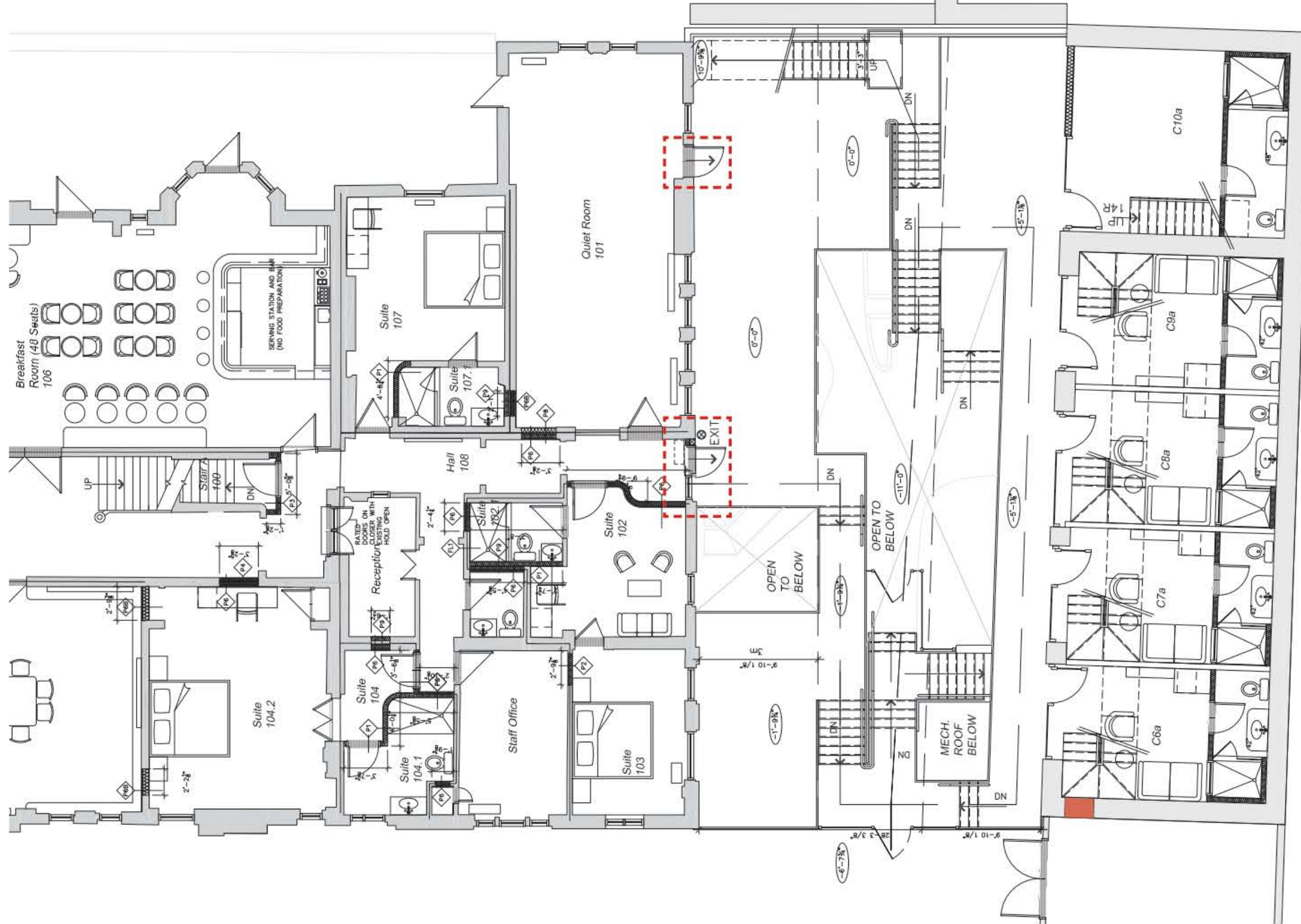
22108-2_CourtyardCarriageHouse_04 2024-01-30



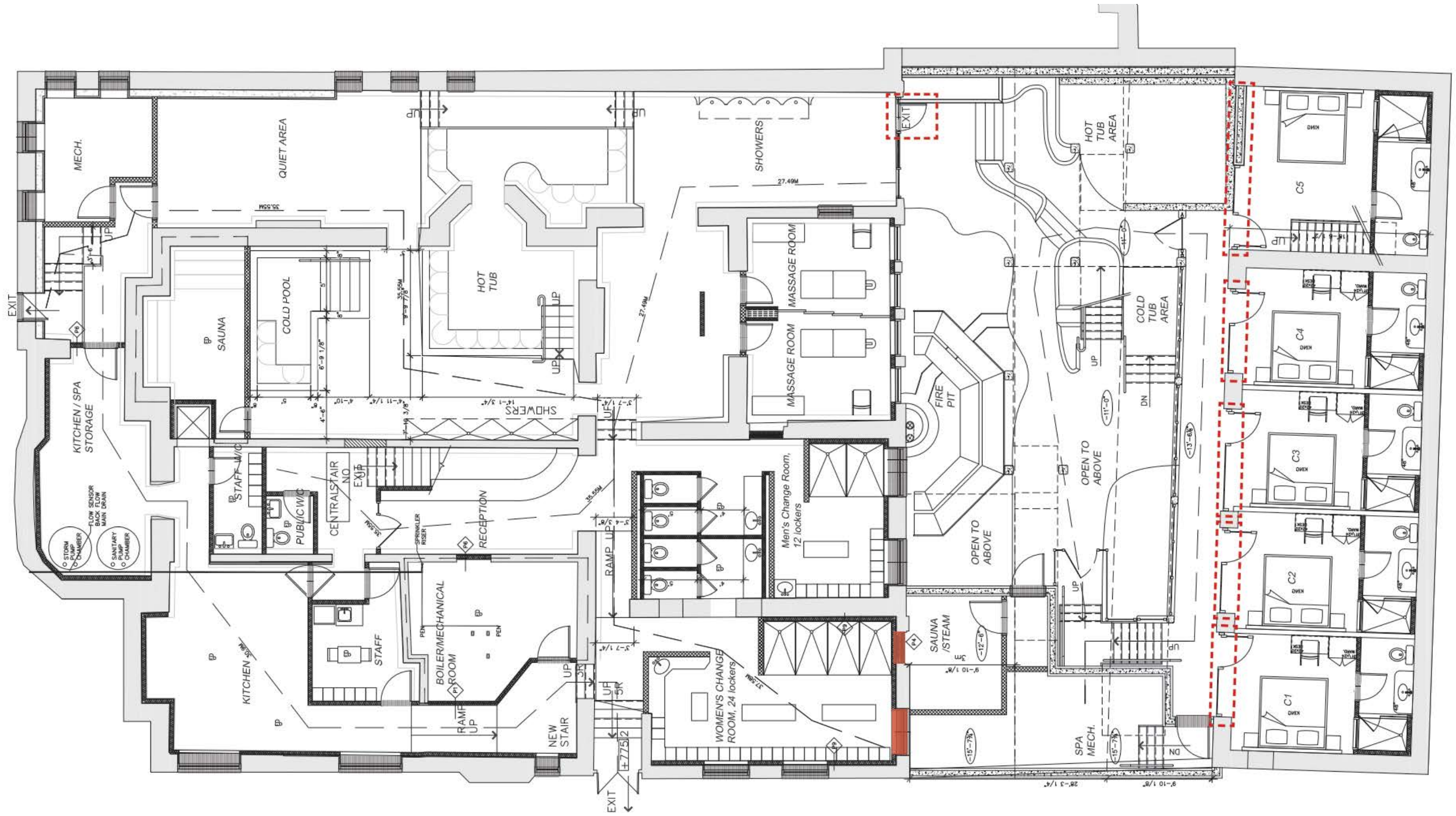
Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.



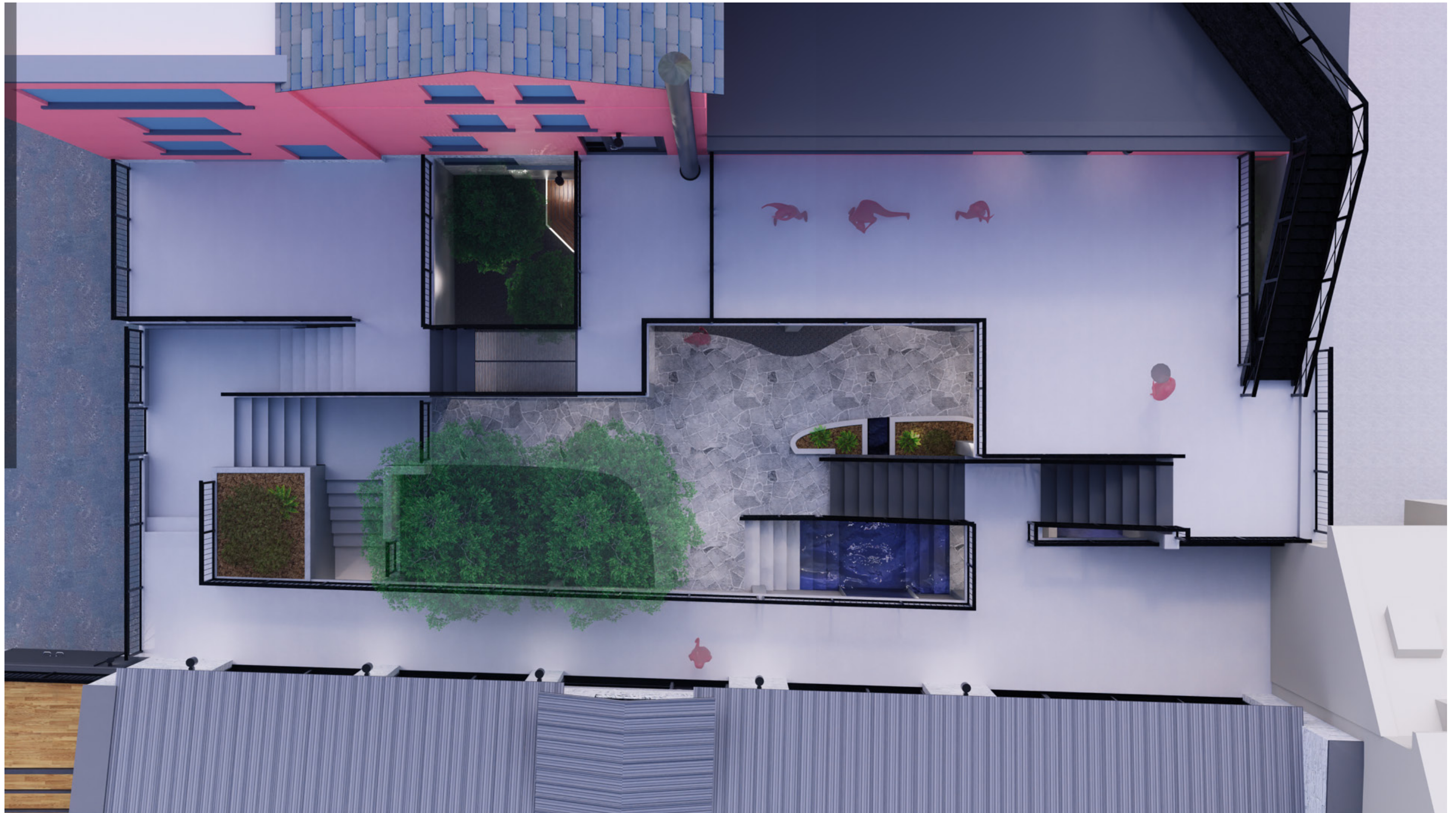
Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.



Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.



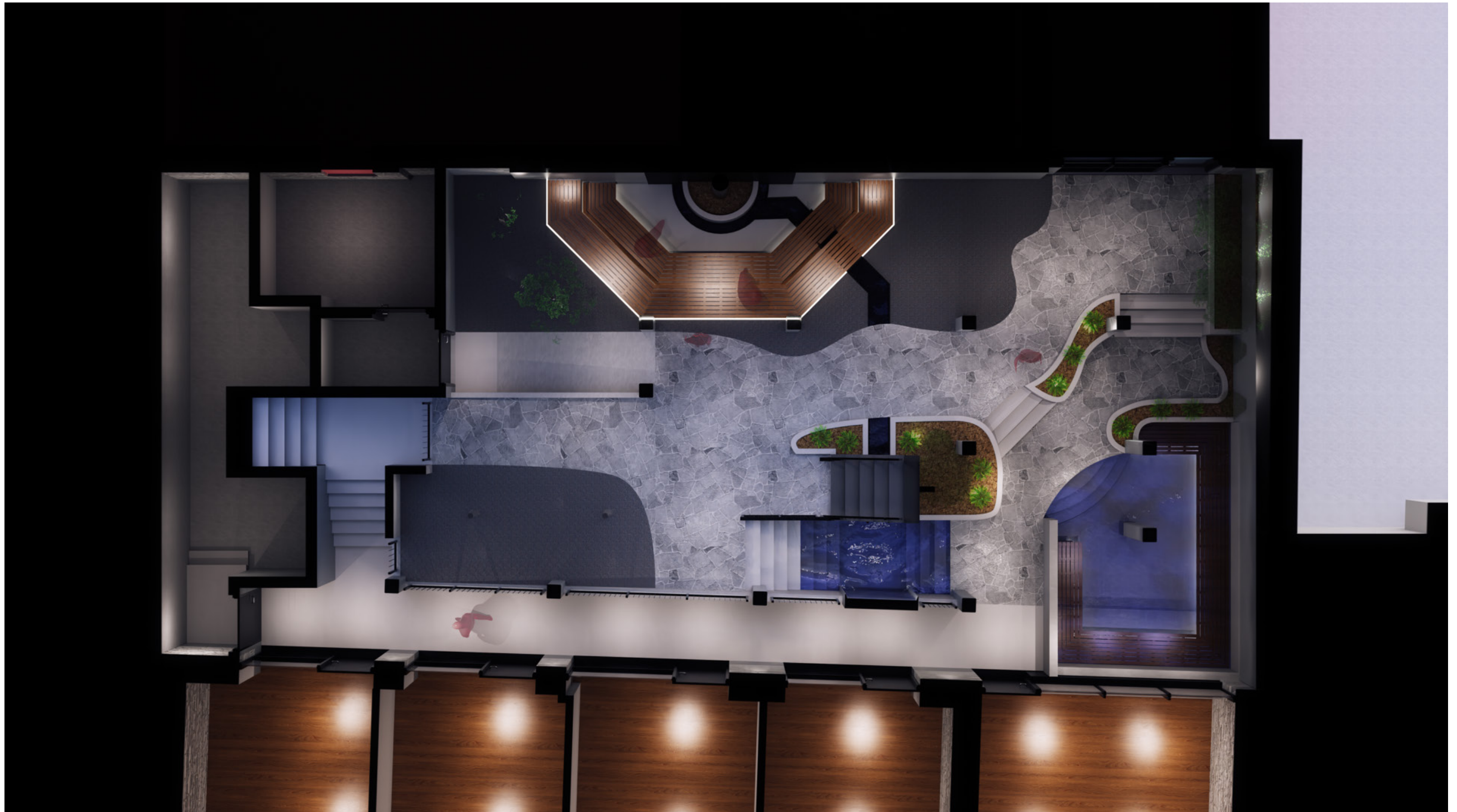
Note: Configuration of outdoor spa amenities such as pools, firepit, vegetation and planting beds are conceptual.



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Notes:

All work to be completed on BPE property with the exception of the re-pointing of the East facing masonry wall of the carriage house and installation of new windows on said wall. BPE has permission from neighbours to access this wall for said work.

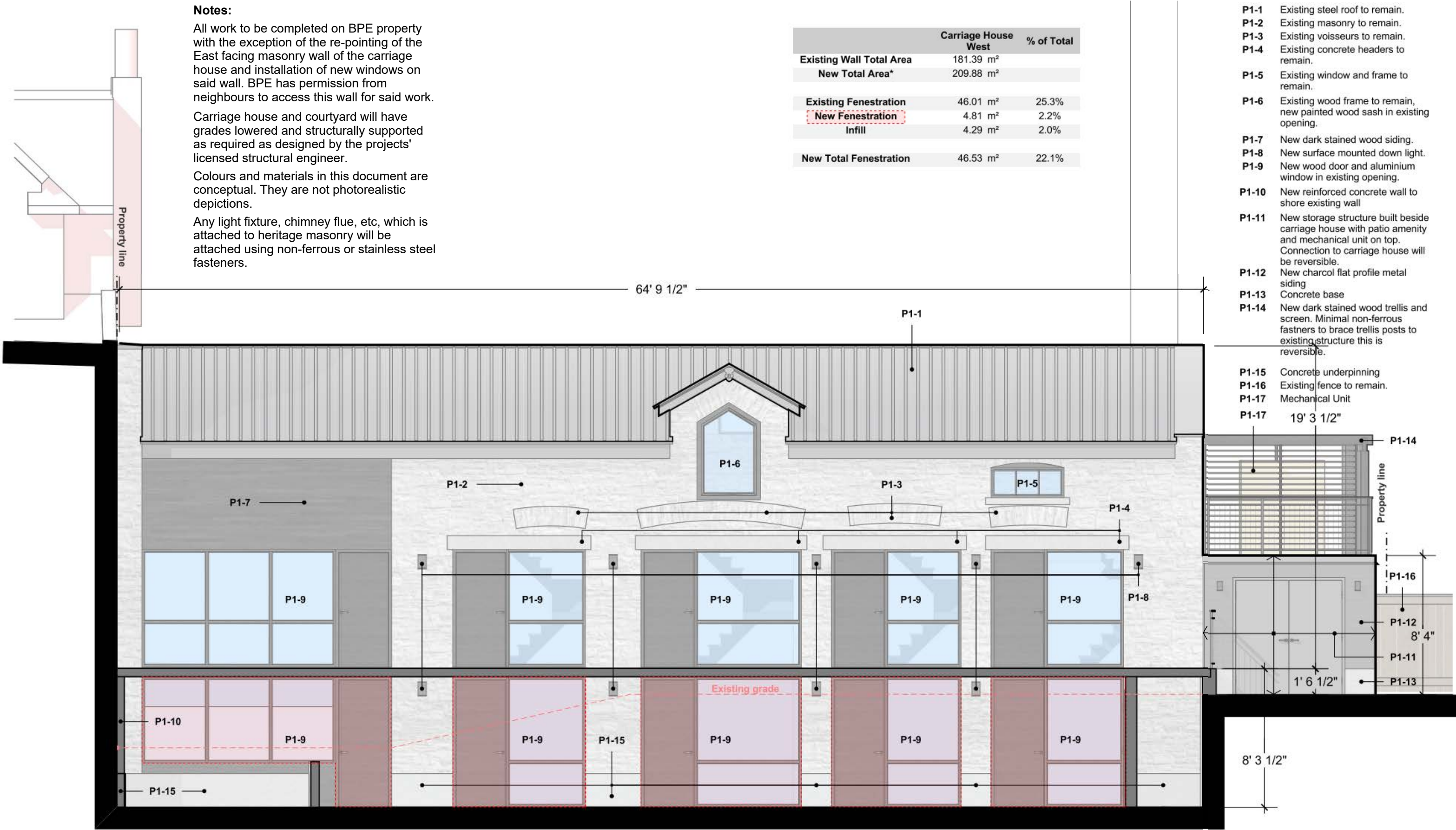
Carriage house and courtyard will have grades lowered and structurally supported as required as designed by the projects' licensed structural engineer.

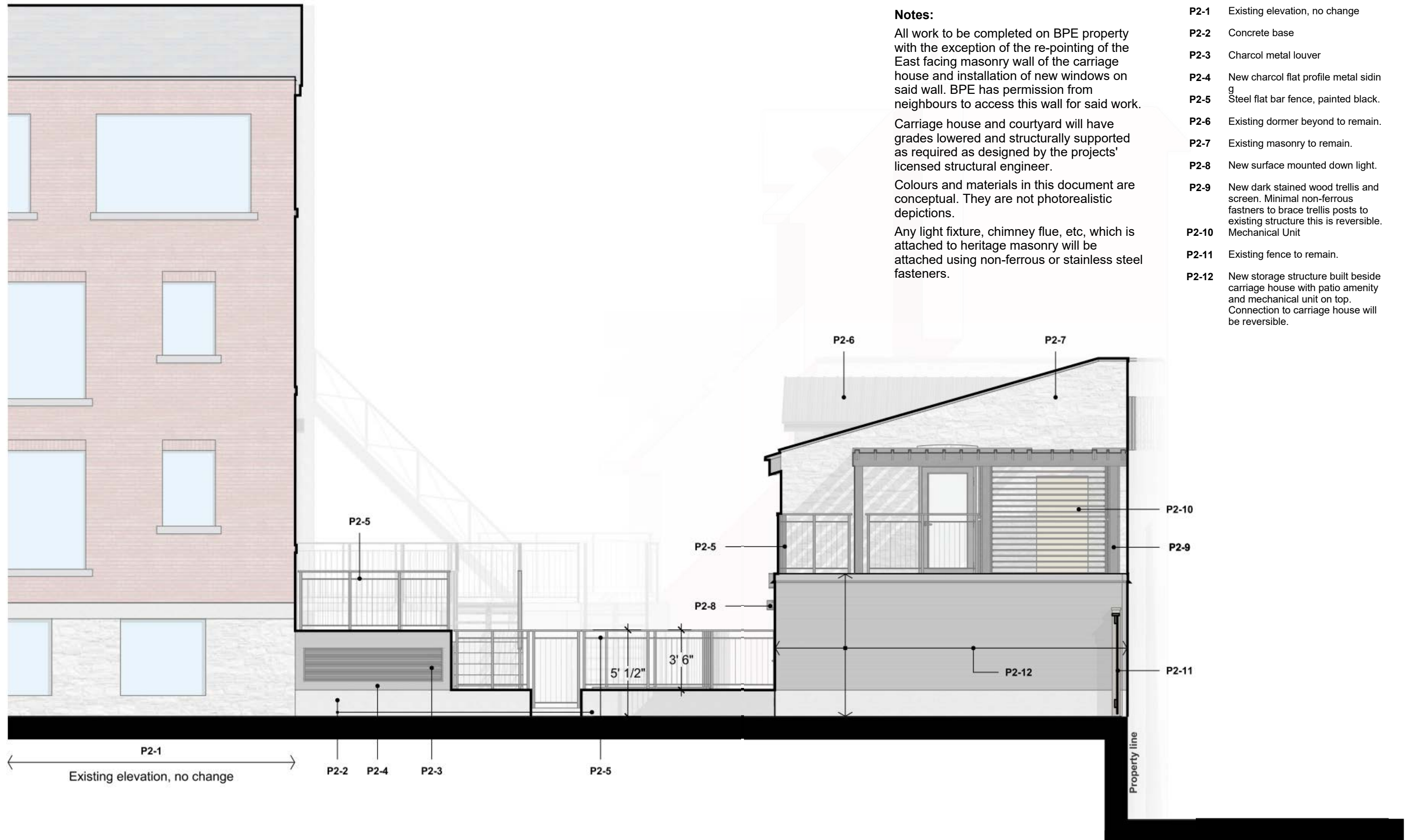
Colours and materials in this document are conceptual. They are not photorealistic depictions.

Any light fixture, chimney flue, etc, which is attached to heritage masonry will be attached using non-ferrous or stainless steel fasteners.

	Carriage House West	% of Total
Existing Wall Total Area	181.39 m ²	
New Total Area*	209.88 m ²	
Existing Fenestration	46.01 m ²	25.3%
New Fenestration	4.81 m ²	2.2%
Infill	4.29 m ²	2.0%
New Total Fenestration	46.53 m ²	22.1%

- P1-1 Existing steel roof to remain.
- P1-2 Existing masonry to remain.
- P1-3 Existing voisseurs to remain.
- P1-4 Existing concrete headers to remain.
- P1-5 Existing window and frame to remain.
- P1-6 Existing wood frame to remain, new painted wood sash in existing opening.
- P1-7 New dark stained wood siding.
- P1-8 New surface mounted down light.
- P1-9 New wood door and aluminium window in existing opening.
- P1-10 New reinforced concrete wall to shore existing wall
- P1-11 New storage structure built beside carriage house with patio amenity and mechanical unit on top. Connection to carriage house will be reversible.
- P1-12 New charcol flat profile metal siding
- P1-13 Concrete base
- P1-14 New dark stained wood trellis and screen. Minimal non-ferrous fastners to brace trellis posts to existing structure this is reversible.
- P1-15 Concrete underpinning
- P1-16 Existing fence to remain.
- P1-17 Mechanical Unit





Notes:

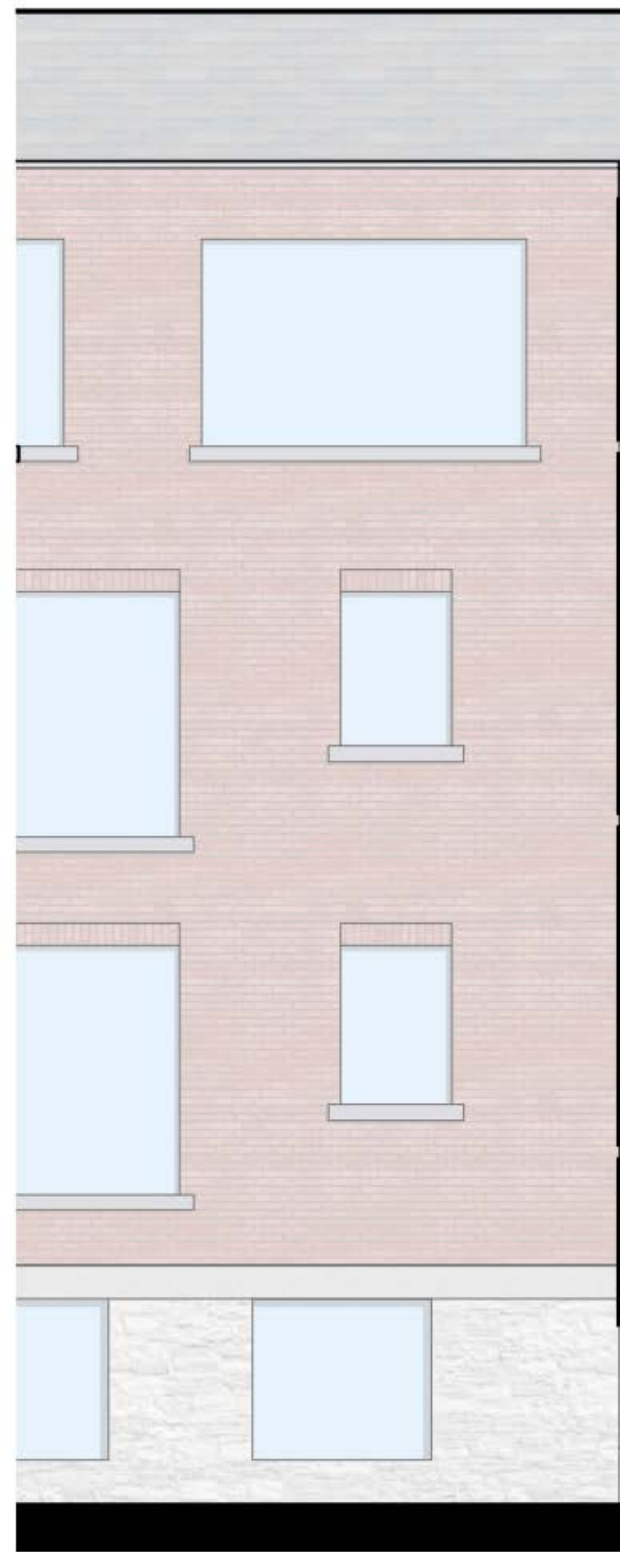
All work to be completed on BPE property with the exception of the re-pointing of the East facing masonry wall of the carriage house and installation of new windows on said wall. BPE has permission from neighbours to access this wall for said work.

Carriage house and courtyard will have grades lowered and structurally supported as required as designed by the projects' licensed structural engineer.

Colours and materials in this document are conceptual. They are not photorealistic depictions.

Any light fixture, chimney flue, etc, which is attached to heritage masonry will be attached using non-ferrous or stainless steel fasteners.

- P2-1** Existing elevation, no change
- P2-2** Concrete base
- P2-3** Charcol metal louver
- P2-4** New charcol flat profile metal siding
- P2-5** Steel flat bar fence, painted black.
- P2-6** Existing dormer beyond to remain.
- P2-7** Existing masonry to remain.
- P2-8** New surface mounted down light.
- P2-9** New dark stained wood trellis and screen. Minimal non-ferrous fasteners to brace trellis posts to existing structure this is reversible.
- P2-10** Mechanical Unit
- P2-11** Existing fence to remain.
- P2-12** New storage structure built beside carriage house with patio amenity and mechanical unit on top. Connection to carriage house will be reversible.



	Carriage House South	% of Total
Existing Wall Total Area	34.5 m ²	
New Total Area*	34.5 m ²	
Existing Fenestration	1.5 m ²	4.4%
New Fenestration	1.4 m ²	4.0%
Infill	0.8 m ²	2.3%
New Total Fenestration	2.1 m ²	6.1%

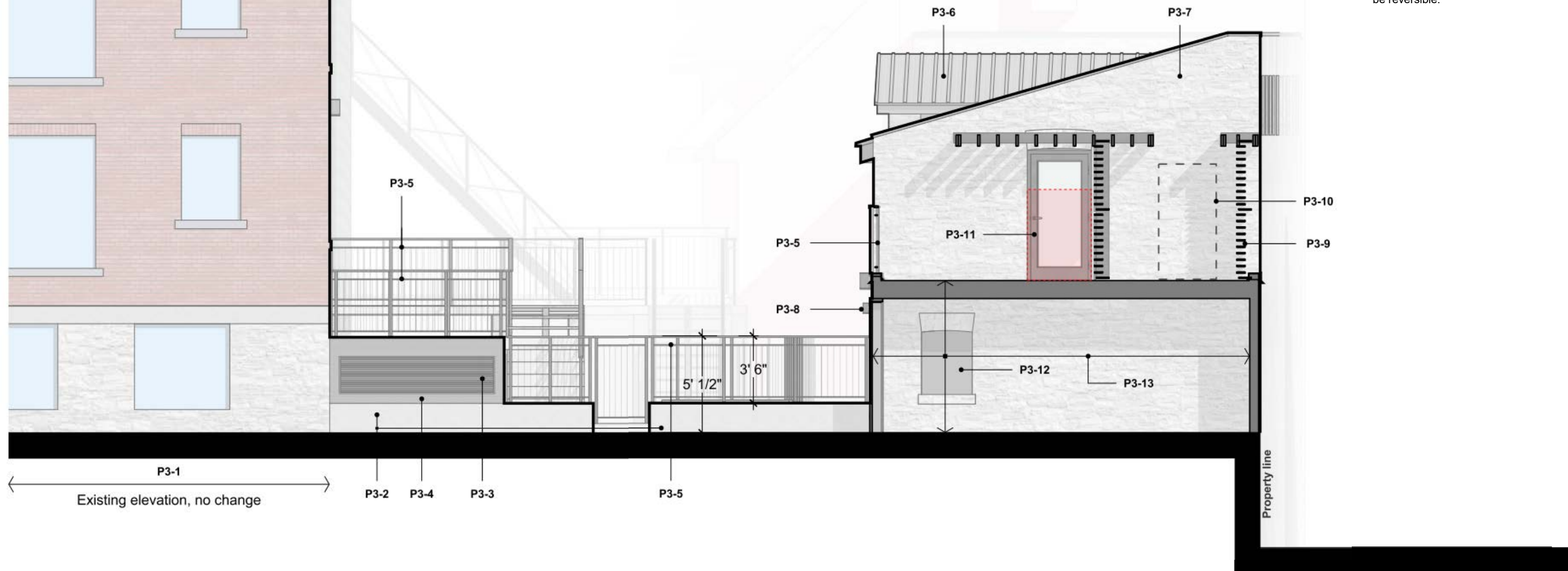
Notes:

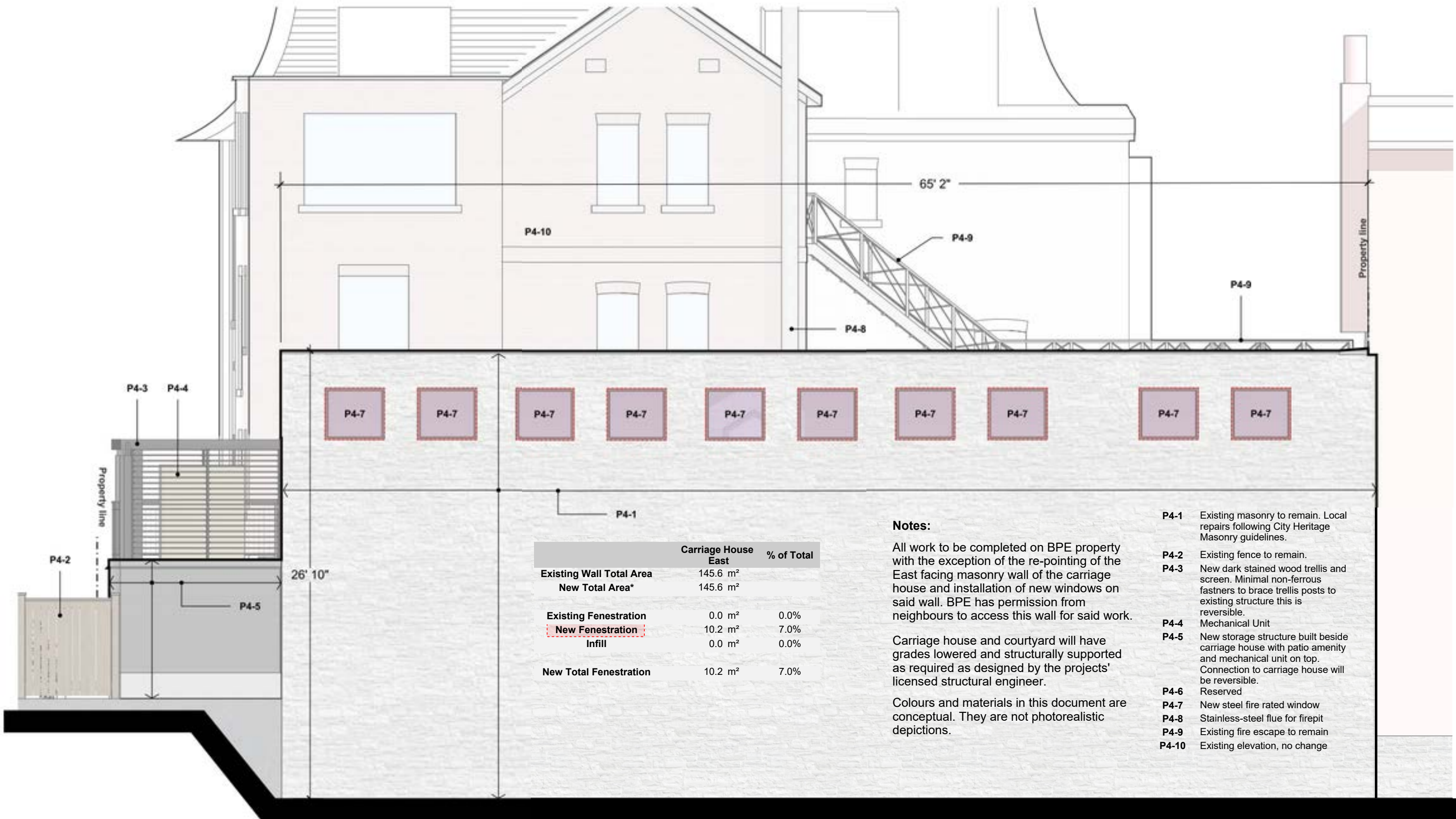
All work to be completed on BPE property with the exception of the re-pointing of the East facing masonry wall of the carriage house and installation of new windows on said wall. BPE has permission from neighbours to access this wall for said work.

Carriage house and courtyard will have grades lowered and structurally supported as required as designed by the projects' licensed structural engineer.

Colours and materials in this document are conceptual. They are not photorealistic depictions.

- P3-1 Existing elevation, no change
- P3-2 Concrete base
- P3-3 Charcol metal louver
- P3-4 New charcol flat profile metal siding
- P3-5 Steel flat bar fence, painted black.
- P3-6 Existing dormer beyond to remain.
- P3-7 Existing masonry to remain.
- P3-8 New surface mounted down light.
- P3-9 New dark stained wood trellis and screen. Minimal non-ferrous fastners to brace trellis posts to existing structure this is reversible. Mechanical Unit
- P3-10 Existing window enlarged into door by lowering sill.
- P3-11 Existing window infilled with charcol metal siding.
- P3-12 New storage structure built beside carriage house with patio amenity and mechanical unit on top. Connection to carriage house will be reversible.





	Carriage House East	% of Total
Existing Wall Total Area	145.6 m ²	
New Total Area*	145.6 m ²	
Existing Fenestration	0.0 m ²	0.0%
New Fenestration	10.2 m ²	7.0%
Infill	0.0 m ²	0.0%
New Total Fenestration	10.2 m ²	7.0%

Notes:

All work to be completed on BPE property with the exception of the re-pointing of the East facing masonry wall of the carriage house and installation of new windows on said wall. BPE has permission from neighbours to access this wall for said work.

Carriage house and courtyard will have grades lowered and structurally supported as required as designed by the projects' licensed structural engineer.

Colours and materials in this document are conceptual. They are not photorealistic depictions.

- P4-1** Existing masonry to remain. Local repairs following City Heritage Masonry guidelines.
- P4-2** Existing fence to remain.
- P4-3** New dark stained wood trellis and screen. Minimal non-ferrous fasteners to brace trellis posts to existing structure this is reversible.
- P4-4** Mechanical Unit
- P4-5** New storage structure built beside carriage house with patio amenity and mechanical unit on top. Connection to carriage house will be reversible.
- P4-6** Reserved
- P4-7** New steel fire rated window
- P4-8** Stainless-steel flue for firepit
- P4-9** Existing fire escape to remain
- P4-10** Existing elevation, no change

- P5-1 Existing window to remain.
- P5-2 Existing window to remain. Film to be added to inside of glazing for privacy.
- P5-3 Brick infill.
- P5-4 Window enlarged to doorway. Brick header to be lengthened to suit new door.
- P5-5 Bricked-in window to be enlarged to doorway. Brick header to be lengthened to suit.
- P5-6 Garage door converted to doorway, glazing and louver.
- P5-7 Door to remain, fixed, not for use.
- P5-8 Stainless-steel flue for firepit
- P5-9 Existing fire escape to remain
- P5-10 Concrete deck
- P5-11 New reinforced concrete wall to shore existing wall
- P5-12 New surface mounted down light.
- P5-13 New firepit
- P5-14 Chimney to be removed (not original to addition).

Notes:

All work to be completed on BPE property with the exception of the re-pointing of the East facing masonry wall of the carriage house and installation of new windows on said wall. BPE has permission from neighbours to access this wall for said work.

Carriage house and courtyard will have grades lowered and structurally supported as required as designed by the projects' licensed structural engineer.

Colours and materials in this document are conceptual. They are not photorealistic depictions.



	Belvedere East	% of Total
Existing Wall	181.3 m ²	
Total Area		
New Total Area*	209.8 m ²	
Existing Fenestration	46.0 m ²	25.3%
New Fenestration	4.8 m ²	2.2%
Infill	4.2 m ²	2.0%
New Total Fenestration	46.5 m ²	22.1%







Submitted by Salex, Inc



Job Name:
Toronto Public Library Wychwood Branch
Renovation & Expansion
Engineer: HH Angus (Ltg) (Don Mills)

Catalog Number:
620-4312-RAL8019

Notes:

Type:
LW2-RAL8019

SALEX18-24175

VLS410 LED

620-4312

1/3



Description

IP66, Class I, IK07. Surface mounted LED wall luminaire. Marine-grade, die-cast aluminum alloy. 5CE superior corrosion protection including PCS hardware. Silicone rubber gasket. Safety glass lens. Suitable for installation over 4" recessed junction box.

Beam Type	symmetric, medium beam [M]
Light Source	LED-3/6W / 700 mA - 3000 K
CRI	80
Gear Type	electronic gear
Nominal Luminous Flux (lm)	
LED Lumens	246 lm
LEDs	3
Total Lumens	738 lm
Tj	85 °C
Delivered Lumens Flux (lm)	
LED Lumens	165.8 lm
Total Lumens	497.5 lm
Ta	25 °C
Rated Input Power	8.5 W

WE-EF LIGHTING USA LLC
410-D Keystone Drive | Warrendale PA 15086 | U.S.A. | Tel +1 724 742 0030 | Fax +1 724 742 0035 | info.usa@we-ef.com | www.we-ef.com | 02-10-2019 10:40

Submitted On: Jan 29, 2019

1/4

[Index Page](#)



H.H. Angus & Associates Ltd. Project Ref.: 2151083-000, Shop Drawing Ref.: GC-E-0005, Date: 2019-11-06, Status: Resubmit Page 9 of 15

The Belvedere - Courtyard and Carriage House - Proposed light and example from other project 04 of 04
Shoalts and Zaback Architects Ltd. **sza**

22108-2_CourtyardCarriageHouse-ExPhotos_01 2024-01-22

----- Original message -----

From: CMO ENG <cmoeng@cogeco.ca>

Date: 2024-01-21 6:21 p.m. (GMT-05:00)

To: Brad Vanderhaar <brad@bpedevelopment.com>

Subject: RE: BPE 141 King Street East, Kingston - Commitment Letter/Engineer On Record

Brad:

This letter is to confirm that CMO Engineering Limited in Trenton Ontario has been retained by BPE for all engineering works associated with the current rehabilitation works at the above named location in Kingston, Ontario. The proposed works include but not limited to the following :

- Structural design
- Field assessment/reviews and instructions
- Construction reviews
- Preparation of construction review reports
- Certification of structural design drawings for Building Permit application
- Day to day structural engineering assignments and supervision associated with the project.

Regards,

Charles C. Onuah, B. Eng., M. Sc., P. Eng.
Senior Structural/Project Engineer

CMO ENGINEERING LIMITED
40 FRANKFORD CRESCENT, UNIT 13
TRENTON, ONTARIO
K8V 4L2

PHONE.: 613-394-3097
FAX.: 613-394-1086

6. Conservation and Development Approach

6.1 Overview of Proposed Development

The proponent wishes to retain the carriage house structure and rehabilitate it for hotel use. The courtyard is to be converted into outdoor amenity space serving spa functions, and the rear elevation of the hotel is to be slightly modified to accommodate new access openings. Please refer to the architectural plans and drawings for details.

Rear elevation (hotel)

Alterations to the rear elevation of the hotel include enlarging a window opening in the central wing to create an exit door to a rear deck (this will involve widening as well as lengthening the opening). This will alter the symmetry of the fenestration pattern on that storey, a minor negative impact on the appearance of this elevation. The dimensions of the other rear elevation existing openings remain unchanged and the enlarged opening represents a very small percentage increase in openings on this elevation. A new opening in the north wing is proposed, again to provide an exit door to the rear deck. The existing wooden panelled door on the ground floor of the west wing will be retained as a decorative feature and will be sealed and insulated on the interior. The exposed stone foundation wall will remain and only a few of the existing openings proposed to be replaced with brick infill (the rest of the existing windows on the lower level and ground floor will remain, the lower ones with privacy film over the glazing).

The existing fire escape will be reconfigured to make use of the new rear deck and lower courtyard. The existing chimney on the one-storey rear addition to the main building is a response to an earlier interior layout and use and is no longer functional. Its removal does not negatively impact the heritage attributes of the main building. A new steel chimney is proposed to extend from the open-air fire pit at the lower level of the spa in the courtyard up to above the gable of the central rear wing of the main building. It will be freestanding save for anchors (non-ferrous) at the upper level. It will be clearly distinct from the rear additions and is a reversible intervention save for the removal of the chimney, all proposed interventions are reversible.

Courtyard

A new deck is to be attached to the rear elevation of the hotel, providing access to the driveway and to the spa level below. The deck will be attached to the existing brick wall with non-ferrous (e.g. stainless steel) fasteners. They will have a minimal impact on the wall masonry and will not spall the brick. The existing fire escape will be reconfigured along the east wall and exit to the new deck. The existing brick chimney on the north addition is proposed to be removed to accommodate new interior uses.

Access to the deck and courtyard will be controlled by a metal fence and gate along the driveway. The boundary fence along the driveway will be black-painted metal with openings between the posts and a low height. The fence and gate restrict access to the spa but provide egress to and visual access from the driveway to the heritage attributes of the rear elevation of the hotel and the north elevation of the carriage house. The end of the laneway will be screened from the adjacent property by the existing addition and the neighbouring yard to the east will be screened by a low wall beneath existing stairs.

Lighting will be unobtrusive and confined to small fixtures required by OBC and located facing downward at the edges of doorways and directed to the underside of the access decks. Light fixtures will face down except for uplighting under the carriage house access deck. Lighting in the outdoor spa will be a combination of bollard lights along pathways and under lights for the access decks.

The grade reduction in the courtyard required to create the spa and the lower hotel of the carriage house exposes parts of the hotel's stone foundation thus providing a more complete understanding of the building's structure. Excavation below the carriage house does not negatively affect that building's heritage attributes as none are impacted. The courtyard will no longer be used as service access to the hotel and vehicular access to the carriage house, thus its conversion to spa uses will change its character. However, its former function is not a heritage attribute.

Carriage house

Changes to the carriage house exterior include modifications to accommodate new interior layouts and floor levels required to create five two-storey hotel. Interventions in the existing fabric include enlarging an existing window on the upper level of the west wall and creating a new opening for an access door. The existing window/door opening on the ground level of the west wall will remain closed. On the north wall, the existing former garage doors will be replaced with new glazing and access doors, all accessed by a new deck running the length of the west

elevation and linked to the main level of the hotel. Below these will be an equivalent set of window/door openings created by excavation of the foundation level and underpinning the existing structure while adding hotel floor area. On the south elevation (facing Ontario Street), there will be small rectangular windows punched into the upper part of the wall, near the top. The existing roof, with its exposed fastener metal-ribbed design, will be retained unaltered, with its existing finish.

On the north elevation, the existing door openings on the ground floor are unchanged, as is the gable end opening (the existing wooden panel infill will be removed and replaced by glazing). New openings below grade are in an area not currently existing and thus do not add to the number of openings on the existing, above-grade building. Openings on the south (Ontario Street) wall remove a small percentage of existing stone in order to accommodate new small windows and metal surrounds. The new doorway opening in the wall facing the driveway is slightly larger than the existing window which will be enclosed, thus making a slight increase in the amount of void in this wall (note that, due to the presence of a large storage container, it is not possible to take current photographs of this elevation: please refer to an historical photograph found on page 6 of the chronology for a partial view, as well as the conjectural drawing) . Both of these interventions are minor and do not have a negative impact on the heritage attributes of the carriage house.

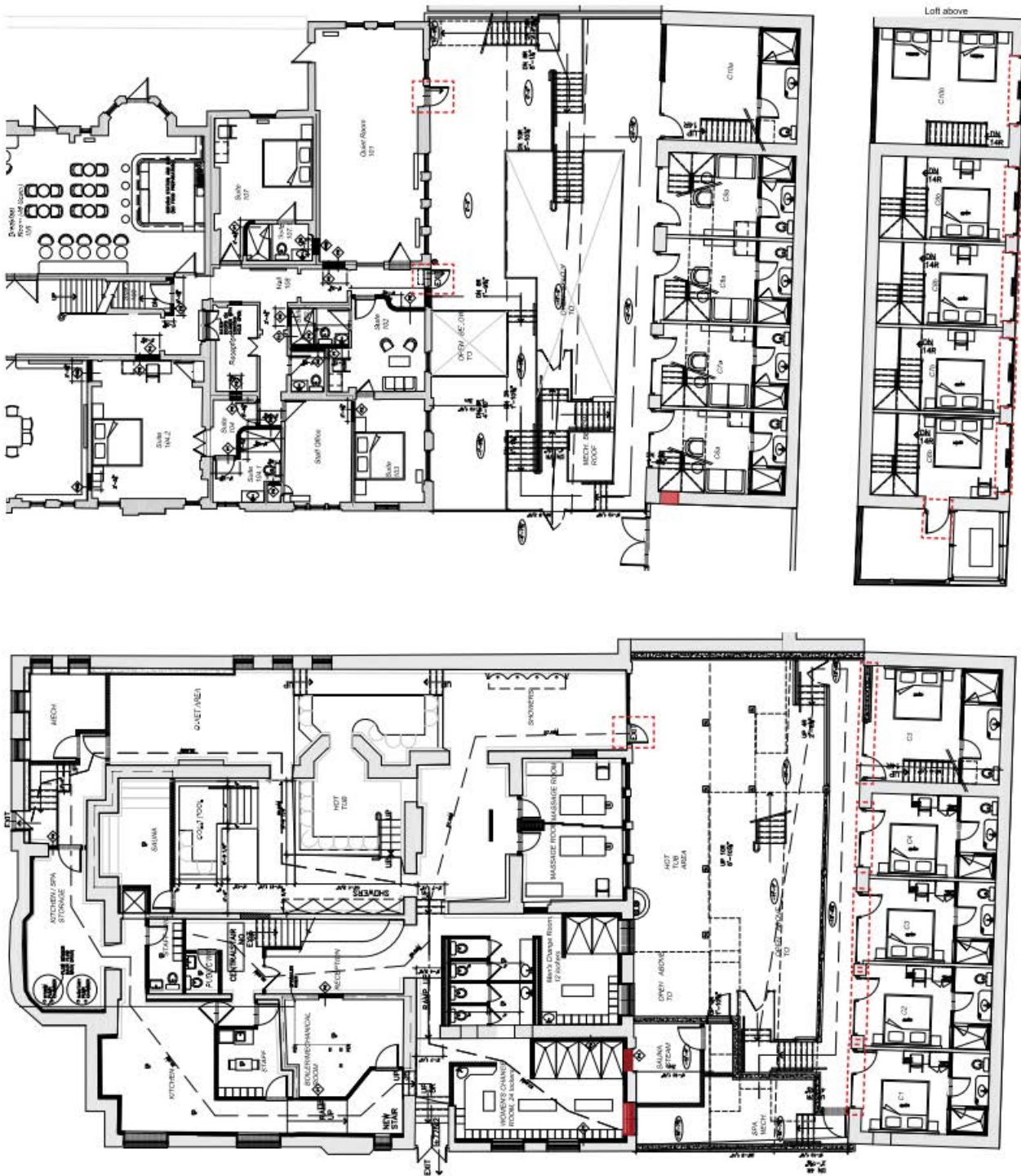
Window types include fire-rated glazing in the south elevation (to meet OBC requirements) and new aluminum, double-glazed doors and windows on the ground floor and sub-floor, on the north elevation facing the courtyard. The existing dormer and shallow-arched windows on the upper storey of the north elevation will have new double-glazed units inserted into the existing wooden frames (which will be repaired and restored, as needed).

A framed addition will be attached to the west wall and contain storage space as well as heating and cooling equipment that would otherwise have to be contained within the heritage building. There will be access to the top of this structure from the adjacent unit. Existing stonework and wooden details on the carriage house will be repaired, as needed. The proposed storage building is visible from the street at the end of the driveway, but only just. It is designed to be unobtrusive, with neutral colours on the wooden cladding and lightly stained wood on the upper deck and pergola. It is clearly distinct from the carriage house. As an addition to the rear of the main building, it is visually secondary to both the hotel and the carriage house. The visual focus within the District is on the streetscape and this addition, visible only along a narrow driveway, will read as an accessory to the primary buildings and public realm that are the key heritage attributes of the Heritage Conservation District. The addition

is a reversible intervention since it is not attached to the carriage house (although it abuts it) and only the upper level pergola will be anchored to the western stone wall (with small metal fasteners that can be removed and the openings repaired).

Materials

The attached architectural drawings label the proposed materials. On the carriage house, cladding material are proposed to be wood, metal and stone, while brick and concrete are proposed for the rear of the main building. These materials are compatible with or similar to those found on the existing buildings and thus do not negatively impact the heritage attributes of the main building or carriage house. The exposed stone foundation wall on the main building will remain and only the existing openings proposed to be closed will have brick infill.



Renderings of the proposed rehabilitation, with red-lined plans showing interventions in the existing building fabric

6.2 Impact and Mitigation

6.2.1 Carriage House

The proposed changes to the above-grade parts are minor and reversible. Small windows in the south wall overlook a parking lot and are small enough to preserve the privacy of occupants of the new residential units and of residents in the apartment building located south of the parking lot. The existing rear wall is part of the carriage house, not a separate wall like many of those described in the District Plan and found as remnants of carriage houses. The fire insurance plans included in the attached chronology, as well as site photos showing outlines of previous structures, confirm that, for much of its history, this part of the carriage house was hidden behind the 2 storey brick structures in the property facing Ontario Street.

On the north elevation, the metal and glass windows and doors that provide visual and functional access to the new residential uses will be distinct from the surrounding stone walls and from the former wooden garage doors, creating a contrast that highlights the existing materials and forms.

The western addition is clearly secondary to the main structure, being lower in height, flat-roofed and clad in flat profile charcoal metal siding. An open wooden pergola structure is proposed to provide shade for a deck atop the addition, allowing views of the stone west wall of the carriage house. The pergola will be attached to the west stone wall with non-ferrous metal fasteners. The upper gable end of the west side of the carriage house is visible over the addition, across the proposed deck and under the proposed roof structure. The existing dormer on the north elevation will remain as a window opening, conserving the wooden frame and detailing in the gable end.

The existing stone structure will be protected during construction and stabilized during excavation for the lower storey units. A structural engineer with experience in heritage structures has been retained and will advise on any detailed structural work required to conserve the integrity of the main building additions and of the carriage house. The addition of a concrete slab under the ground floor of the carriage house and the extension downwards of the existing wall sections between the door openings would appear to be sufficient to provide structural support. Subject to any forthcoming recommendations from the structural engineer, at this point a Temporary Protection Plan does not seem to be required. See the excerpt below from CMO Engineering's email of 21 January, 2024 to BPE confirming the engineer's involvement:

This letter is to confirm that CMO Engineering Limited in Trenton Ontario has been retained by BPE for all engineering works associated with the current rehabilitation works at the above named location in Kingston, Ontario. The proposed works include but not limited to the following:

- Structural design
- Field assessment/reviews and instructions
- Construction reviews
- Preparation of construction review reports
- Certification of structural design drawings for Building Permit application
- Day to day structural engineering assignments and supervision associated with the project.

Regards,

Charles C. Onuah, B. Eng., M. Sc., P. Eng.

Senior Structural/Project Engineer

CMO ENGINEERING LIMITED

40 FRANKFORD CRESCENT, UNIT 13

TRENTON, ONTARIO

K8V 4L2



Subgrade conditions

6.2.2 Hotel and Landscape

Alterations to the rear elevation of the hotel are minor and reversible. As this elevation includes three additions to the original house, and has been modified since the time of each wing's construction, new interventions will be a continuation of this pattern of subsequent interventions that respond to changing interior functions and configurations. Repairs to the existing stonework and brickwork will be made, as needed. Repairs will be completed in conformity with City's masonry conservation standards. Two existing openings on the rear elevation of the main building will be closed on the foundation level (the existing wooden door will be removed: it is not of heritage value) and the remaining windows covered with privacy film on the inside glazing. Openings above in the ground floor will be retained and the existing paneled wooden door will be retained in situ and sealed and insulated on the inside. One existing window opening will be enlarged to provide an exit door and another opening created in the east wing for the same purpose. By retaining most of the existing openings, the existing fenestration pattern will be clear, and each of these changes will be a reversible intervention.

Excavation of the rear yard will not impact heritage attributes of the adjacent structures provided that the buildings and boundary wall are protected during construction. Following completion of construction there will be opportunities for interpretation of the history of the hotel complex in the publicly accessible portions of the courtyard and along the proposed wall bordering the shared driveway. Any type of interpretation (e.g. plaques/panels) should be publicly accessible and thus would be best located along the King Street side of the property, preferably in front of or affixed to the wall of the terrace that extends to the east of the main building. Content of the plaque or panel will be subject to discussion with, and approval by, City heritage staff and heritage advisory committee. Archaeological assessment has been deemed unnecessary by the City (the courtyard has been disturbed over time as a result of successive alterations and additions to the rear elevation of the hotel).



Subgrade conditions on hotel and courtyard

6.2.3 Adjacent Heritage Properties

Due to the existing side and rear additions on the hotel and the east and south boundary walls that are part of the carriage house structure, very few of the proposed alterations will be visible from adjacent heritage properties. The proposed rear deck will be screened from view from the east by a low wall along the top of the existing concrete block boundary wall next to the fire escape and most of the below-grade spa activities, as well as the lower level of the residential units, will be largely screened from view by overhead decks that provide access and egress to the hotel and carriage house. The carriage house and its proposed western addition will be largely hidden from public view, with those views restricted to any that can be seen down the shared driveway.

For the boundary wall with No. 155 King Street West, subject to the detailed design provided by the project structural engineer, this concrete block wall will be underpinned alongside the excavated portion of the courtyard. The stone east wall of the carriage house will remain intact as the structural support for the building and will not impact the abutting wall of No. 155.

All work will be confined to the proponent's property with the exception of the south wall discussed below. The existing board fence at the end of the driveway is on the neighbour's property, as shown in the revised drawings supplied by the proponent. The proponent indicates that adjacent owner to the south has verbally granted access to the rear wall of the carriage house to allow construction of the proposed windows in the upper storey.

A formal letter from the adjacent owner will be prepared confirming the applicant's ability to access the rear (south) elevation of the carriage house from the adjacent property.



View west from No. 155 King Street East



View west of rear of No. 131-33 King Street East



View south along driveway



View from the street


7. Conclusions and Recommendations

The proposed alterations and additions to the hotel complex conserve a heritage property and rehabilitate it for commercial and residential use. Repairs to the hotel exterior follow good conservation practice and include restoration of missing or deteriorated elements. This work is in accordance with the intent and requirements of the heritage easement. Interventions in the rear courtyard and carriage house, while not addressed in either the designation by-law or heritage easement, are also proposed to follow good conservation practice.

To summarize the impact of the proposed alterations, this HIS has identified more detailed heritage attributes for the carriage house and the proposed design has conserved these attributes. The proposed changes are minor interventions in the stone and frame walls and are reversible: the proposed western addition is likewise removeable in future.

The rear courtyard does not have heritage value and its excavation will have no direct impact on the heritage attributes of the adjacent hotel and carriage house. The hotel's stone foundation will be fully visible under a rear deck and the proposed lower level of the carriage house will continue the structural and fenestration pattern of the north wall of the existing carriage house. Recommendations for protection of heritage attributes during construction include reference to a structural engineer's assessment of the existing buildings and to any temporary protection plans that they might recommend. The City has not required an archaeological assessment.

In conclusion, it is my professional opinion that the proposed alterations and additions to the rear of the hotel property, the courtyard and carriage house adequately address the heritage attributes of the property and follow good conservation practice. The design, as shown in the plans and drawings included in this HIS, demonstrate the ways in which they do so.



Carl Bray PhD CAHP MCIP RPP

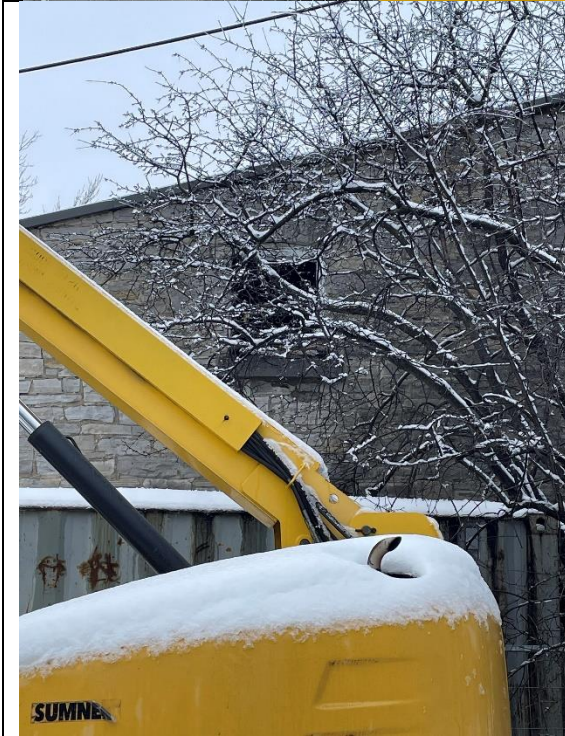
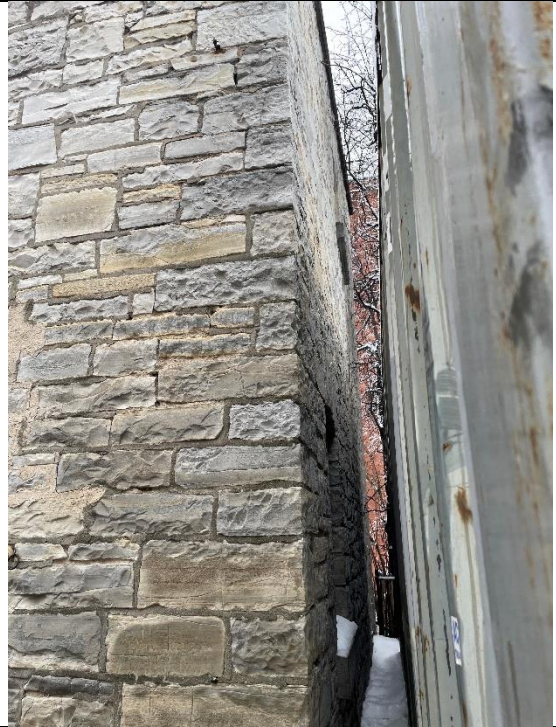
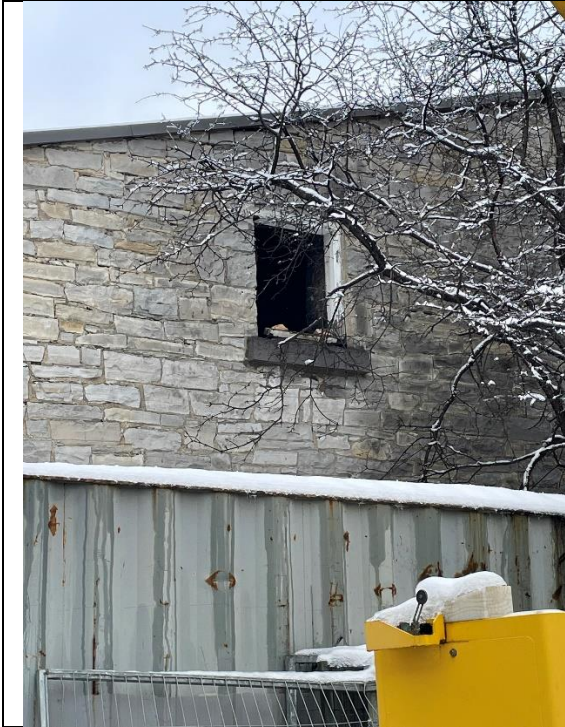
Staff Site Visit Photos 1-25-24:













Kingston Heritage Properties Committee

Summary of Input from Technical Review Process

P18-004-2024

Committee Members	Comments Enclosed	No Comments Provided	No Response Received
Councillor Glenn			X
Councillor Oosterhof			X
Jennifer Demitor			X
Gunnar Heissler	X		
Alexander Legnini			X
Jane McFarlane	X		
Ann Stevens	X		
Peter Gower	X		
Daniel Rose			X



where history and innovation thrive

City of Kingston
216 Ontario Street
Kingston, Ontario
Canada, K7L 2Z3

Website: www.cityofkingston.ca

TTY: Dial 613-546-4889

Date:	January 23, 2024
Form:	Heritage Kingston Reviewer Form
Reviewer Name:	Peter Gower
Application Type:	Heritage Permit
File Number:	P18-004-2024
Property Address:	141 King Street East

Description of Proposal:

This proposal is to renovate the rear elevation of the former hotel and restore deteriorated components of the exterior and to convert the former carriage house into 10 hotel units. The major components of this proposal include adding a storage shed that will abut the rear stone carriage house and adding a porch/overhang at the base of the existing main openings facing into the rear yard. Further, the proposal calls for renovating/regrading (by digging below existing grade) the rear yard to allow for a fire pit, various staircases, a hot tub/spa area and clear access to the hotel building. This proposal also entails new openings in the carriage house (facing Ontario Street and the rear yard) and enlarging openings on the rear of the hotel building.

Comments for Consideration on the Application:

Because of weather, snow and ice conditions, and because of ongoing construction and destruction work on the site, I found it impossible to seriously estimate the impact of the proposed changes. I therefore have to rely on the comments on page 25 of the HIS which assures us that there will be no serious detrimental effects to the heritage aspects of the property. I certainly understand the argument here, and hope that it is unflawed.

Recommended Conditions for the Application:

I hope that heritage staff will keep a close watch on what actually happens to ensure that heritage attributes are not damaged or lost.



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City of Kingston
216 Ontario Street
Kingston, Ontario
Canada, K7L 2Z3

Website: www.cityofkingston.ca

TTY: Dial 613-546-4889

Date:	January 23, 2024
Form:	Heritage Kingston Reviewer Form
Reviewer Name:	Ann Stevens
Application Type:	Heritage Permit
File Number:	P18-004-2024
Property Address:	141 King Street East

Description of Proposal:

This proposal is to renovate the rear elevation of the former hotel and restore deteriorated components of the exterior and to convert the former carriage house into 10 hotel units. The major components of this proposal include adding a storage shed that will abut the rear stone carriage house and adding a porch/overhang at the base of the existing main openings facing into the rear yard. Further, the proposal calls for renovating/regrading (by digging below existing grade) the rear yard to allow for a fire pit, various staircases, a hot tub/spa area and clear access to the hotel building. This proposal also entails new openings in the carriage house (facing Ontario Street and the rear yard) and enlarging openings on the rear of the hotel building.

Comments for Consideration on the Application:

This project seems to be well-planned. The hotel had become rather shabby and almost neglected so the restoration of the front will be a welcome sight along this part of King Street. I am supportive of this project.

I am also impressed by the planning for the rear yard and the stone carriage house which now will be restored. About the windows, other members of the Heritage Properties Committee have more experience than I have. I like what has been proposed.

My major concern is the stone foundation and the backing wall of the carriage house. From the rear view from the apartment parking lot, the stone wall looks so precarious. The stone house that collapsed a few years ago on Princess Street comes to mind. I'd like to know that a structural engineer will be required to examine that wall's stability.

Recommended Conditions for the Application:

Structural engineer analysis that the stone wall will not collapse.



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Date:	January 27, 2024
Form:	Heritage Kingston Reviewer Form
Reviewer Name:	Gunnar Heissler
Application Type:	Heritage Permit
File Number:	P18-004-2024
Property Address:	141 King Street East

Description of Proposal:

This proposal is to renovate the rear elevation of the former hotel and restore deteriorated components of the exterior and to convert the former carriage house into 10 hotel units. The major components of this proposal include adding a storage shed that will abut the rear stone carriage house and adding a porch/overhang at the base of the existing main openings facing into the rear yard. Further, the proposal calls for renovating/regrading (by digging below existing grade) the rear yard to allow for a fire pit, various staircases, a hot tub/spa area and clear access to the hotel building. This proposal also entails new openings in the carriage house (facing Ontario Street and the rear yard) and enlarging openings on the rear of the hotel building.

Comments for Consideration on the Application:

visual appearance of the carriage house onto Ontario Street and prevention of accelerated deterioration of the wall after the proposed construction has been completed! It has been mused by people who know that Kingston has consistently failed to make the best of its heritage inventory, but rather it puts it away for posterity. The notion of preservation overshadows the expectation to preserve together with harmony and the creation of beauty. The insertion of the 10 minimalistic square windows as expressed on the drawings may be rationalized as being minimally invasive on the heritage wall that is already not attractive. It is now about to be made much less attractive and the opportunity to create beauty would be lost (at the least until it is hidden by a butting building that may be constructed at some future date. The architect should be challenged to be creative in the treatment of the wall by using the windows as jewels! Finishing the carriage house; heating and cooling it, will subject the heritage wall to freezing temperatures at the dew points and weather penetrations. Note that the wall is oriented to the prominent weather. Accelerated deterioration is to be expected unless preventative measures are incorporated in the proposed construction. It is not evident

that an effort has been made by the applicant to protect the longevity of the wall with appropriate sealing, barriers(liquid, vapour and air), and ventilation.

Recommended Conditions for the Application:

The applicant should be required to provide detailed descriptions of measures of preservation of the subject wall.



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Date:	February 1, 2024
Form:	Heritage Kingston Reviewer Form
Reviewer Name:	Jane McFarlane
Application Type:	Heritage Permit
File Number:	P18-004-2024
Property Address:	141 King Street East

Description of Proposal:

This proposal is to renovate the rear elevation of the former hotel and restore deteriorated components of the exterior and to convert the former carriage house into 10 hotel units. The major components of this proposal include adding a storage shed that will abut the rear stone carriage house and adding a porch/overhang at the base of the existing main openings facing into the rear yard. Further, the proposal calls for renovating/regrading (by digging below existing grade) the rear yard to allow for a fire pit, various staircases, a hot tub/spa area and clear access to the hotel building. This proposal also entails new openings in the carriage house (facing Ontario Street and the rear yard) and enlarging openings on the rear of the hotel building.

Comments for Consideration on the Application:

This proposal raises issues that, although falling under the purview of Planning/Site Alteration/Engineering/OBC/Minor Variance etc, can impact on the Heritage value of the property and will be addressed from that standpoint, for example, making additional openings in the external building fabric to accommodate more doors and venting.

Courtyard:

Although this proposal refers to excavation, from the photos it appears that excavation has already taken place. This extensive partially completed and proposed excavation poses some concerns addressed in Parks Canada Standards & Guidelines for the Conservation of Historic Places in Canada, 4.1.4 Spatial Organization and 4.1.5 Visual Relationships, particularly referencing new features that alter and obscure spatial organization and alter the visual relationship of the space with new features that are incompatible in size, scale, material, style and colour. Historically this area/courtyard behind the house and in front of the former stables/garages would have been near or at grade and simple in nature allowing easy foot access to and between both the house and carriage house. In general, what is proposed for the courtyard appears overwhelming for the small space and incompatible in size, scale and design. The large modern deck on stilts attached to the house is unsympathetic as are the similar decks

on the carriage house. The former open space is taken up with firepit, hot tub, cold tub and sauna. While the use of the courtyard area for these amenities poses practical issues regarding lack of green space and parking, too much hardscaping, snow removal and drainage, more historically suitable for this area would be some green space and gardens with path to move between the two buildings. Reducing these outside amenities or incorporating the spa facilities into the basement interior would maintain the historic aspect and character of the exterior space and allow for year-round use of the amenities.

The extensive excavation also necessitates the use of too many unsympathetic square metal railings that also overwhelm the space and both buildings.

Carriage House:

It should be noted that masonry repairs are subject to heritage permit approval.

The design of the carriage house proposal removes any sense of its former function and the excavation, railings, and deck on stilts raise the same concerns about spatial organization and visual relationships regarding the courtyard. The proposal for the carriage house is incompatible in size, scale and design to both it and the main building.

It alters the visual relationship with the main building, the courtyard and the carriage house itself. More sympathetic to the carriage house exterior while maintaining its historic proportions and its relationship to the house and courtyard would be to reduce the depth of excavation and have the entrances slightly below or at grade with a half set of stairs down inside and up inside from grade. This sort of "garden flat" has been successfully implemented in other locales, preserves the sense of entering at ground level, could provide easy access to the courtyard and would eliminate the deck on stilts.

Recognizing that heritage wall assemblies are unique and must be specifically designed to ensure the preservation of the masonry, it will be important that the heritage wall assembly retrofit is designed and reviewed by a heritage engineer and preservationist.

Support of the carriage house assembly during construction may be necessary.

Converting this carriage house into conditioned living space will also require the use of suitably designed HVAC systems which will require exterior venting as will bathroom venting. Penetrations for venting on the exterior of the carriage house need to be carefully planned to minimize impact on the exterior of the carriage house from all elevations.

The square looking proposed new window openings on the carriage house are not particularly compatible and it would make sense, if allowable on the lot line, that any proposed new openings should allow for air movement. Despite this, proposed windows should give the impression of more vertical than horizontal or square, possibly 2 over 2 or 2 over 3 with external muntins. Window details could be informed by other examples of windows in carriage houses of this era.

The doors for the carriage house entries would be more suitable if they could give the sense of its past as such.

The new storage structure is very utilitarian in design and materials, uncomplimentary to the carriage house and if it can be viewed from King St or neighbouring properties unsympathetic to the Old Sydenham HCD. Its design, which necessitates the infill of an existing window should be reconsidered, in order to maintain the existing opening and light into the carriage house.

Main Building:

Best practice would maintain all openings as they exist. Elongating a window as a door is acceptable within the header space but the door that has a side glass panel necessitating widening the opening should be reconsidered and altered. P5-4 should be designed to the same dimensions as P5-5.

Avoiding the brick infill of a basement window and door is also best practice and if the sauna/steam room and spa mechanical were moved inside or redesigned this might be accomplished.

The firepit chimney seems unsuitable and a distraction for the main building.

There is no indication of the necessary HVAC, kitchen and bathroom venting for the main building. Penetrations such as this need to be carefully planned to minimize their impact on the heritage building envelope.

Recommended Conditions for the Application:

Any windows to be removed should be assessed and preserved on site for reversibility purposes along with any brick and stone being removed.

Photo documentation of existing conditions should take place prior to any more construction, excavation or renovation.

Summary of Final Comments at the February 21, 2024 Heritage Properties Committee Meeting

[To be added following the meeting.]